

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	16 th May 2018
	REPORT OF:	HEAD OF PLACES & PLANNING
	AUTHOR:	Billy Clements
	TELEPHONE:	01737 276087
	EMAIL:	billy.clements@reigate-banstead.gov.uk
AGENDA ITEM:	5	WARD: Merstham

APPLICATION NUMBER:	17/02542/F	VALID:	13 November 2017
APPLICANT:	Bellway Homes (South London) & SAL Pension Fund	AGENT:	Savills
LOCATION:	BELLWAY HOUSE, 241 LONDON ROAD NORTH, MERSTHAM		
DESCRIPTION:	Demolition of existing buildings and the erection of 33 apartments (including affordable housing), provision of car parking, amenity space and associated infrastructure.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

SUMMARY

This is a full application for the demolition of Bellway House and the erection of a building comprising 33 apartments (10 affordable) with associated car parking and amenity space.

The scheme has been developed through extensive pre-application negotiation with Officers, including the Conservation Officer given the proximity to Merstham Conservation Area, and further marked improvements to the footprint, massing and height of the northern end of the building have also been secured during the course of the application in order to reach what is considered to be an acceptable solution.

The resultant building takes the form of a linear block along the full length of frontage onto the adjoining A23, following the siting of the existing office building in terms of its building line onto Station Road North. The height, scale and massing at the corner with Station Road North would be near identical to the existing office building and the building would be four storeys as it moves north along the A23, this part of the site is significantly lower than the adjoining road, such that the perceived scale of the building would be a storey or more lower than its true height and would not appear unduly dominant or out of scale. Whilst the scheme would present a relatively long elevation onto London Road North, the design and articulation is considered to be successful in breaking this elevation up into a series of elements - through the use of projecting gables, variation in eaves and ridge height and a varied but appropriate palette of materials – in order to respond to the scale, grain and character of the Conservation Area and surrounding street scene. The revised layout and footprint negotiated during the course of the application would provide scope for amenity space and landscaping within the site which would be a benefit compared to the existing

hard landscaped dominated car parking. The Conservation Officer raises no objection to the proposals in terms of impact on nearby heritage assets.

Given the separation distances involved and design/layout of the site, the proposals are not considered to give rise to amenity concerns for neighbouring properties, particularly when compared to the existing situation and that which could arise through the fall-back prior approval conversion.

A total of 21 parking spaces are proposed to serve the development, served by the existing access from London Road North. Whilst the parking is below the local maximum standards, the application is supported by analysis of local car ownership data to justify the level of parking and proposes provision and membership of a car club for future occupants and improvements to local bus facilities. Taking the above into account and mindful of the advice and response of the Highway Authority, it is considered that the development would be balanced in favour of sustainable travel (as required by both local policy and the Framework) and that the proposed parking level – whilst below local maximum standards – would not give rise to highway or other harm which would warrant refusal.

Under Core Strategy policy, the development should provide on-site affordable housing at a rate of 30% of the proposed dwellings. In this case, the scheme provides the full 10 units which would be required by this policy, with a mix of 1 and 2 bedroom units. Whilst the affordable housing units would be all shared ownership tenure (rather than a mix as advised in the Affordable Housing SPD), the applicant has provided evidence from registered providers that a mixed tenure would not be attractive in this case given the number units proposed.

The proposals would result in the loss of employment floorspace; however, the principle of residential conversion has already been established through a previous prior approval application which is considered to be a realistic fall-back. Refusal on this point is not therefore considered to be sustainable.

The scheme would contribute to meeting local housing requirements, including affordable housing, and would bring consequent social, economic and financial benefits all of which weigh in favour of the proposal. Whilst the scheme would represent a more dense use of the site than present, with the improvements made to the massing and footprint of the building, it is considered that it strikes an appropriate balance between making optimal use of a brownfield site whilst also preserving and enhancing the character of the area and adjoining heritage assets.

RECOMMENDATION(S)

Subject to the completion of all documentation required to create a planning obligation under Section 106 of the Town and Country Planning Act 1990 (as amended) to secure:

- (i) 10 units of affordable housing (shared ownership tenure)
- (ii) Provision of one car club vehicle on-site or in an otherwise accessible location to the development for a minimum of two years, with all costs associated with the provision of the vehicle including parking space and pump priming being met by the developer

- (iii) Free car club membership for all households for a period of two years with the equivalent of 25 miles free drive-time credit for residents of the proposed development using the car club vehicle
- (iv) The Council's legal costs in preparing the agreement;

Planning permission is **GRANTED** subject to conditions.

In the event that a satisfactorily completed obligation is not received by 31 July 2018 or such longer period as may be agreed, the Head of Places and Planning be authorised to refuse permission for the following reason:

The proposal fails to make adequate provision for affordable housing and is therefore contrary to policy CS15 of the Reigate and Banstead Core Strategy 2014.

Consultations:

Conservation Officer: Originally raised concerns regarding the height and massing of the north end of the building (particularly in winter views), the low ratio of parking and proposed improvements to the north bound bus shelter on High Street. Based on the amended plans, raises no objection subject to conditions to control external details, finishing materials and bus stop improvement details.

County Highway Authority: No objection subject to conditions. Response contains the following commentary:

"I can confirm that the proposed replacement of the existing B1 office use on this site with a residential development is acceptable in principle, given its location in the centre of Merstham. The existing access to the site is located off Station Road North, which is a private road. For this reason, the County Highway Authority (CHA) would only really be concerned with the impact of the proposed development at the point where Station Road North meets the public highway, at its junction with the A23 High Street/London Road North. Nevertheless, I am satisfied from the TRICS data output provided that the proposed residential development would generate less vehicular trips per day than the existing office use, including during the AM and PM peak hours. The site access is therefore considered to be adequate to serve the proposed development.

With regard to car parking, the proposed provision of 23 car parking spaces for 39 apartments equates to a ratio of 0.59 spaces per unit. This is not ideal, given that the site is not located within or close to a town centre. However, the CHA will only raise objections to a shortfall in parking if it is considered that the shortfall would lead to danger on the adjoining public highway. In this case, the site is situated in a relatively accessible location, within 150m of Merstham railway station, and within 200m of the bus stops on High Street.

Furthermore, there are double yellow line waiting restrictions on Station Road North and the A23 High Street / London Road North in the vicinity of the site, which would prevent on street parking from taking place in locations where it would be considered dangerous. For these reasons, the CHA would be unlikely to object to the proposed level of on-site car parking provision for the development. With regard to turning, I am satisfied based on the vehicle tracking plan provided that a 2.5m by 10.3m refuse vehicle would be able to turn around within the site and exit in a forward gear."

Tree Officer: No objection subject to conditions – comments as follows:

"The arboricultural report produced by Simon Jones Associates (SJA air 17063 -01a dated November 2017) demonstrates the redevelopment of this site can be completed without the need to remove any trees. In addition, there is unlikely to be significant post development placed on the trees which often leads to pressure to remove or prune trees which degrades their natural appearance. Therefore, based on the existing information I support this application subject to the following conditions being attached to the decision notice."

Contaminated Land Officer: Identifies potential for ground contamination to be present on and/or in close proximity to the site and therefore recommends conditions.

Surrey Lead Local Flood Authority: Comments outstanding

UK Power Networks: No objections

Environmental Health – Air Quality: No objection subject to conditions

Surrey Crime Prevention Design Adviser: Objects due to lack of information and reference to security or creation of a safe environment [informative proposed].

Representations:

In respect of the original plans, letters were sent to neighbouring properties on 22nd November 2017; a site notice was posted 6th December 2017 and the application was advertised in local press on 7th December 2017.

Letters were sent to neighbouring properties in respect of the revised plans on 28th March 2018 and a site notice posted on 4th April 2018.

One response was received in relation to the original plans and a further two responses following re-consultation on the amended scheme. The following issues were raised:

Issue	Response
Inadequate parking	Paragraphs 6.24 to 6.29, conditions 13, 14, 15 and 16 and proposed heads of terms for planning obligation (as above)
Increase in traffic and congestion	Paragraphs 6.24 to 6.30, conditions 13, 14, 15 and 16 and proposed heads of terms for planning obligation (as above)
Hazard to highway safety	Paragraphs 6.24 to 6.30, conditions 4 and 13
Overdevelopment	Paragraphs 6.3 to 6.15
Poor design	Paragraphs 6.3 to 6.15
Inconvenience during construction	Paragraph 6.22 and condition 4
Noise and disturbance	Paragraph 6.22
Crime fears	Paragraph 6.45
No need for the development	Paragraph 6.42 - each case on its own merits
Property devaluation	Not a material planning consideration

1.0 Site and Character Appraisal

- 1.1 The site comprises a part 2, part 2.5 storey office block of traditional pitched roof design, situated at the corner of Station Road North with the A23/High Street. The office block was extended in the early 2000s. There is also a garage outbuilding in the north east corner of the site.
- 1.2 The site is set down at a lower land level than the adjoining A23, and slopes such that the northern part of the site is lower than the frontage on Station Road North. The boundary along the A23 is formed by hedging and post and rail fencing, giving

way to denser tree cover and screening to the more northern reaches of this boundary.

- 1.3 The surrounding area is characterised by a mix of uses including residential (which includes the two dwellings immediately adjacent to the site to the east) along with retail, pub and community uses consistent with the designation of the adjoining areas as a Local Centre. There is a telephone exchange and depot (used for bus storage) adjoining the site to the north east.
- 1.4 The site is immediately adjacent to the Merstham Conservation Area which covers buildings on the opposite side of A23/High Street and the opposite side of Station Road North, including the Grade II listed Feathers public house. The special interest of the Conservation Area derives from its cohesive traditional village format, complemented by later arts and crafts development. There are a number of other statutory and locally listed buildings in the immediate vicinity of the site, including the Old Fire Station opposite on Station Road North.
- 1.5 As a whole, the application site extends to approximately 0.20ha.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: Pre-application advice relating to the redevelopment of the site was sought earlier this year. Advice was given in respect of the form and design of the buildings, including in relation to the adjoining heritage assets.
- 2.2 Improvements secured during the course of the application: Amendments to the roof form and reduction in the height of northern end of the building and reduction in the width of the rear return leg (by 11.9m) (resulting in a reduction from 39 units to 33 units) with associated additional landscaping and ground floor amenity space Changes to fenestration to improve relationship to The Old Sweet Shop.
- 2.3 Further improvements to be secured through planning conditions or legal agreement: Various conditions are recommended to control landscaping, materials and other works to ensure a high quality development. Conditions to obscure glaze particular windows to safeguard neighbour amenity. Conditions to secure highway requirements, including bus stop improvements, are also recommended. A legal agreement will be required to secure the on-site affordable housing provision.

3.0 Relevant Planning and Enforcement History

- | | | | |
|-----|----------------|--|--|
| 3.1 | 17/01771/PAP3O | Notification of proposed change of use of class b1(a) office to class c3 (dwellings house) consisting of 15no. flats. | Prior approval not required
25 September 2017 |
| | 16/01312/CLE | Planning permission was originally granted in 1986 with a number of conditions including a restrictive condition on the type of occupier. The certificate is required to clarify the | Approved
22 July 2016 |

current use of the site as an unfettered office (b1a)

- 3.2 There is other planning history associated with the use of the site as offices (including Advertisement applications); however, these are not considered to be relevant to this case.

4.0 Proposal and Design Approach

- 4.1 The proposed development seeks planning permission for the demolition of the existing dwellings on the site and the erection of a building comprising 33 one and two bedroom with associated parking and communal gardens.
- 4.2 The replacement building would be a single block largely following the building line and siting of the existing office building, but extending along the full depth of the sites frontage along the A23. The building would be a mixture of 3 and 4 storeys (partly reflected the change in levels across the site) and for the most part the top storey would be set partially or wholly within the roof.
- 4.3 The building would be of traditional form and design with a mixture of hipped and gable roof forms and employment gabled projections. The predominant materials palette would be brick and tile hanging, with some accents of render and timber boarding.
- 4.4 A parking area would be created within the site, served by a sloped access which would be in the same position as that which serves the existing offices. Parking would be a mixture of surface spaces and undercroft bays. Communal gardens would be created in the north-east corner of the site.
- 4.5 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.
- 4.6 Evidence of the applicant's design approach is set out below:

Assessment	This site is in a village setting on the edge of Merstham Village Conservation Area. The site itself is a corner site, with a long and narrow shape running along the edge of the London Road and then opening out to the rear behind the Old Sweet Shop and Old Post Office. The levels vary across the site, dropping by c.3m from station road north. Buildings on the opposite side of London Road North are few in number but include the Georgian pub (Railway Arms) and the short terrace with Paxton Watson entrance arch. To the south is the Feathers Hotel which is one of the principal
------------	---

	<p>buildings in the village with an arts and crafts frontage contributing to the impression of the historic village centre.</p> <p>Mature tree planting along the verge also provides a screen between the site and the dual carriageway. These trees are off site and it is important that any new building does not undermine these trees.</p>
Involvement	<p>Pre-application advice was sought from the Council in early and the design evolved in response. A public consultation meeting was held in October 2017 with c.400 leaflets delivered locally, 19 people attended. Comments received through the public consultation have been incorporated into the design. The only concerns noted were a preference for a mix of houses and apartments and concerns regarding parking provision.</p>
Evaluation	<p>The D&A identifies this as a challenging site with steep changes in level away from the road, limited direct frontage and access onto the street. The design concept was for a terrace of buildings along London Road extending the established building line with a 3 storey building at the front and 4 storey building to the rear. This was evolved in response to pre-application advice which raised concerns about the scale of buildings, eaves height and roof form as well as advising that a more traditional approach with more direct reference to the Conservation Area would be preferred.</p>
Design	<p>The applicant's justification for the chosen design is that it makes best use of an accessible brownfield site. The building makes use of the steep fall in the land levels across the site to provide a lower ground floor to the rear and space for undercroft parking. The proposed development retains the established building line onto Station Road and London Road North extending this line along London Road to create a new terrace of buildings along the north west edge of the site. The arrangement of the buildings is configured to avoid overlooking from the new apartments onto neighbouring properties. The design ethos was to break the building down into small block to reflect the scale and character of the Conservation Area, but with a more contemporary appearance. The scale has been designed to respect views around the site and from the Conservation Area.</p>

4.7 Further details of the development are as follows:

Site area	0.20ha
Existing use	Offices (B1(a))
Proposed use	Residential
Net increase in dwellings	33
<i>Of which affordable housing</i>	10
Proposed site density	165 dwellings per hectare (dph)

Density of the surrounding area	112dph – Fintrax House/Stephenson Place 90dph – Station Road North (west side)/Station Road (north side) 70dph – A23 (opposite site) inc. Old Mill Lane 40dph – Quality Street (east side)
Proposed parking spaces	21
Parking standard	BLP 2005 – 47 (1 space per 1 bed and 1.5 spaces per 2 bed)
Estimated CIL contribution	c.£185,000 (pre-indexation and any affordable housing relief)

5.0 Policy Context

5.1 Designation

Urban Area
Adj. to Merstham Village Conservation Area
Flood Zone 1

5.2 Reigate and Banstead Core Strategy

CS1 (Presumption in favour of sustainable development)
CS4 (Valued townscapes and historic environment)
CS5 (Valued people and economic development)
CS10 (Sustainable development),
CS11 (Sustainable construction),
CS12 (Infrastructure delivery)
CS13 (Housing delivery)
CS14 (Housing needs of the community)
CS15 (Affordable housing)
CS17 (Travel options and accessibility)

5.3 Reigate & Banstead Borough Local Plan 2005

Landscape & Nature Conservation	Pc4, Pc2G
Heritage	Pc8, Pc9, Pc10, Pc13
Housing	Ho9, Ho10, Ho13, Ho16
Employment	Em1A
Movement	Mo4, Mo5, Mo6, Mo7
Utilities	Ut4

5.4 Other Material Considerations

National Planning Policy Framework
National Planning Practice Guidance

Merstham Conservation Area Appraisal

Supplementary Planning Guidance

Developer Contributions SPD
Affordable Housing SPD
Local Distinctiveness Design Guide
Surrey Design

Other

Human Rights Act 1998
Community Infrastructure Levy Regulations
2010 (as amended)
Conservation of Habitats and Species
Regulations 2017
Planning (Listed Buildings and
Conservation Areas) Act 1990

6.0 Assessment

- 6.1 The application site is situated in the urban area and comprises of an existing office block and its large car parking area. The site is adjacent to the Merstham Conservation Area.
- 6.2 The main issues to consider are therefore:
- Loss of employment
 - Design and impact on the character of the area, including Conservation Area
 - Effects on the amenity of neighbouring properties
 - Access, parking and highway implications
 - Trees and landscaping
 - CIL and infrastructure contributions
 - Other matters

Loss of employment

- 6.4 The site currently comprises an office block which is in active use. However, as set out in the planning history above, the recent prior approval application (17/01771/PAP30) has established that the existing offices and surrounding curtilage could be converted to residential flats through permitted development. This is a significant material consideration.
- 6.5 Whilst this application would technically conflict with Policy Em1A, the permitted development route is considered to be a realistic fall-back position such that objection to the loss of the offices is not considered to be sustainable and justifies departure from this policy. It is also noted that the emerging Development Management Plan proposes to allocate this site for residential development; however, given the stage of progression, the prior approval fall-back is considered to be the weightier consideration.
- 6.6 In addition, the applicant notes that – even if the PD fall-back did not exist – the existing building is inefficient and dated and would require significant investment to attract another employment use (the inference being that viability of continued employment use is questionable).

- 6.7 Given the established prior approval fall-back, it is considered that refusal of the proposals on the basis of loss of employment would not be sustainable. In coming to this view, regard has also been given to paragraph 51 of the Framework which is supportive of residential redevelopment of commercial sites where there are not “strong economic reasons” why such development would be inappropriate.

Design and impact on the character of the area

- 6.3 The building itself takes the form of a largely linear block along the full length of frontage onto the adjoining A23, following the siting of the existing office building in terms of its building line onto Station Road North. Whilst the proposed building would be tight to the north-eastern boundary of the site, due to the depth of the adjoining highway bank, the building would retain a generous set back from the road and would not appear unduly prominent in this respect.
- 6.8 The portion of the building fronting Station Road North would adopt the same height, scale and massing as the office building presently on site – three storeys with the top storey being wholly within the roof. This approach is considered to help ensure that the building would continue to appear appropriately subservient and recessive in the backdrop to views out of the adjoining Conservation Area and the listed Feathers Public House).
- 6.9 Behind this frontage element, the building would increase to four storeys, making use of the falling land across the northern end of the site. However, at this point, the site is significantly lower than the adjoining A23, such that - in most views along this key thoroughfare into the Conservation Area – the perceived scale of the building would more than a storey lower than its true height. As a result, it is considered that it would not appear out of scale with, or excessively tall in relation to its surroundings. The top floor would again be wholly with the roof which further helps to achieve an acceptable townscape impact on the approach to the Conservation Area.
- 6.10 During the course of the application specific concerns were raised with the applicant regarding the height, footprint and massing of the northernmost end of the building. In response, this element of the building has been significantly amended. The original submission included a full four storey element on the northern corner with a ridge height significantly above the majority of the building, giving this element undue prominence, particularly in winter views when the adjoining tree screen is sparser. By setting the top storey of this part of the building into the roof (as per the rest of the building) and through design changes, this element of the building has been significantly reduced in height (by approximately 3.7m). The changes also allow for a gently staggered ridgeline, helping to break up the apparent length of the elevation whilst also giving the impression of scale building towards the main village. The Conservation Officer has confirmed that he has no objection to the scale and massing of the building from the perspective of impact on the Conservation Area or nearby listed buildings.
- 6.11 The footprint and extent of built form has also been reduced markedly during the course of the application. The initial plans included a long return “leg” on the building which ran across the full width of the northern end of the site, leaving

limited open space – particularly given its size – and giving the perception of the building filling the plot when viewed in behind buildings on Station Road North and thus appearing as an overdevelopment of the site. Through the amendments negotiated with the applicant, this rear “leg” has now been largely removed (reduced in width by nearly 12m and resulting cumulatively in a reduction from 39 to 33 units) such that overall, when coupled with the reduced height – the building is now felt to fit more comfortably within the site and achieve an appropriate balance between built form and landscaping.

- 6.12 Whilst a single block would occupy the London Road North frontage, the design and articulation is considered to be successful in breaking this elevation up into a series of elements - through the use of projecting gables, variation in eaves and ridge height and a varied but appropriate palette of materials – in order to respond to the scale, grain and character of the Conservation Area and surrounding street scene. In terms of materials, the proposed palette of predominantly brick and clay tile with elements of traditional cream/white render and timber boarding is considered to reflect local distinctiveness. The Conservation Officer has recommended a condition to control various architectural details and materials to ensure a high quality development – given the prominent location of the site and its relationship to the Conservation Area, this level of specificity is considered reasonable, necessary and appropriate.
- 6.13 Access to the site would be taken from broadly the same point on Station Road North as the existing site, serving a car parking area to the rear comprising of a mixture of surface and undercroft car parking spaces. Whilst the development would be a comparatively dense use of the site – particularly compared to existing – there would nonetheless be ample opportunity for landscaping to be introduced given the reduced footprint discussed above. Unlike the current arrangement, the scheme would allow for some landscaping along the access road as – including along the boundary with The Old Sweet Shop – which will help soften this presently quite urban feature, whilst also providing scope for soft landscaping and tree planting within the parking areas and communal amenity space.
- 6.14 The one remaining aspect of concern raised by the Conservation Officer relates to suggested improvements to the bus stops in Merstham, particularly the significant engineering works suggested for the north bound stop which is sited on the old village green. Any forthcoming improvements to the stop would need to be sensitively designed, taking account of this historic interest and the Conservation Area more generally. A condition requiring details of the bus stop improvements to be submitted and approved prior to implementation would ensure that the Borough Council would have control over future design through the planning process.
- 6.15 Overall, whilst the scale, massing and built footprint would be greater than the existing office building, with the improvements secured during the course of the application, it is considered that the proposal would respect the character of the area and create an appropriate frontage on the approach to Merstham Village. Furthermore, it is concluded that the proposals would not cause harm to the setting of adjoining heritage assets, including the Conservation Area. The proposal is therefore considered to comply with policies Pc9, Pc13, Ho9, Ho13 and Ho16 of the 2005 Borough Local Plan and policies CS4 and CS10 of the Core Strategy.

Effects on the amenity of neighbouring properties

- 6.16 Two residential properties – The Old Sweet Shop and The Old Post Office – adjoin the site on Station Road North. The next nearest residential properties are on the opposite side of London Road North.
- 6.17 The Old Sweet Shop is a single two storey residential dwelling with a modest rear garden, recently converted from former commercial premises. Whilst the proposed building would extend deeper into the site than the existing offices, the height, scale and massing of the proposed building adjacent to this neighbour and their garden area would not be dissimilar to that of the existing office building. Given this, and mindful of the separation which would be retained between the proposed building and this neighbour's boundary (c.6.5m), it is not considered that the building would give rise to an unacceptable overbearing or overshadowing effect for this neighbour.
- 6.18 In terms of overlooking, the proposal would have windows facing towards The Old Sweet Shop. Given the position of these windows, views towards the rear windows of The Old Sweet Shop would be at an oblique angle and thus would not cause unacceptable mutual overlooking. Whilst some habitable residential windows in the proposed building would potentially have views over the garden area of The Old Sweet Shop, given many of the existing office windows similarly afford such views and mindful of the fact that the building could reasonably be converted to residential through the recently approved prior approval, the current proposal is not considered to result in a harmful loss of privacy when compared to the existing/potential situation.
- 6.19 Unlike the current situation, the scheme would also provide a landscaped buffer between the access road and the flank wall of The Old Sweet Shop, which – together with the reduced parking – may offer benefit to this neighbour in terms of vehicle noise and disturbance.
- 6.20 Turning to The Old Post Office, given the separation distances involved, it is not considered that the building would cause an overbearing or overshadowing effect on this neighbour. Mutual views between residential windows would be at significant distances (minimum 17.5m) and at acute angles. Whilst there may be some overlooking of the outdoor area of The Old Post Office, this would again be at distances and – as the building is split into flats – it is considered to be less sensitive than a private residential garden.
- 6.21 The residential properties on the opposite side of London Road North (Ivor House/Ivor Villas), these would be approximately 25m distant and separated from the site by the main road such that they would not experience an unacceptable loss of amenity.
- 6.22 Concerns have been raised in relation to noise and disturbance. In this respect, introduction of a residential development on this urban site, in an existing local centre location and close to major road infrastructure, is not – in itself – considered to be incompatible or objectionable in terms of potential noise and disturbance for neighbours. As above, other legislative regimes would protect neighbours from

unneighbourly or anti-social behaviour of new occupants. A construction transport management plan is proposed to be secured through condition which would help to control any disturbance or disruption in this respect.

- 6.23 On this basis, whilst giving rise to a degree of change in relationship to surrounding properties, the proposal is not considered to give rise to any seriously adverse impacts on neighbour amenity and therefore complies with policies Ho9, Ho13 and Ho16 of the Borough Local Plan 2005.

Accessibility, parking and highway implications

- 6.24 As discussed above, the development would be accessed from Station Road North, with the access road sited in broadly the same position as existing. Parking for 21 vehicles (equivalent to 0.64 per unit) would be provided through a combination of surface parking bays and undercroft spaces.
- 6.25 The applicant argues – through their Transport Statement – that the proposed level of parking is appropriate taking account of the sustainable location of the site (close to train station, bus services and the shopping parade) and the policy thrust towards reducing private car reliance. Whilst the nearby local centre offers relatively limited shops and services to meet day-to-day needs, it is agreed that the site benefits from relatively high accessibility to public transport which would offer direct access to nearby major centres such as Redhill. In addition, applying typical levels of car ownership amongst flat dwellers in the Merstham area (which taken from the 2011 Census and suggests 0.96 cars per household for owner-occupied flats/0.53 for rented/shared ownership tenure) and taking account of the effect of the proposed car club space (for which they cite evidence demonstrating that access to/membership of a car club reduces car ownership levels by a third), the applicant concludes that parking demand from the original 33 unit scheme would be 18 spaces, i.e. less than the proposed 21 spaces. If the impact of the car club provision is disregarded, the car ownership data cited by the applicant would suggest a requirement for 27 spaces.
- 6.26 Mindful of the reduced level of parking, the County Highway Authority has requested that provision is made for both cycle parking, provision of a car club and an allowance for free membership and “drive time” for future occupants of the development as well as improvements to nearby bus stops. Given the location of the site, nature of the surrounding area and the below standard level of car parking proposed, all of the above requirements are considered to be necessary in order to ensure that there is a variety of sustainable travel options available such that not owning a car would be a realistic option and alternative. Furthermore, the County Highway Authority have confirmed that the extent of parking restrictions in the surrounding area (on Station Road North, High Street and London Road North) would prevent displacement on-street parking from taking place in locations where it would give rise to a dangerous situation or compromise highway safety.
- 6.27 Subject to securing the measures set out above and mindful of the advice of the Highway Authority regarding highway safety, it is considered that the development would be balanced in favour of sustainable travel (as required by both local policy

and the Framework) and that the proposed parking level – whilst below local maximum standards – would not give rise to harm which would warrant refusal.

- 6.28 According to the applicants Transport Statement, using industry recognised data and modal share information from the Census the proposal is calculated to result in a negligible difference with regards to the number of overall trips associated with the site and a reduction in the number of private car trips compared to the existing offices. For this reason, it would be unlikely to result in unacceptable traffic or congestion.
- 6.29 As above, the access point from Station Road North would be broadly located in a similar position to the existing office access. Although Station Road North is a private road, the County Highway Authority has confirmed that – given the access is broadly similar to existing and mindful of the likely reduction in vehicular movements – the access is considered to be adequate to serve the proposed development. The applicant has provided in their Transport Statement vehicle tracking which demonstrates that both a refused vehicle and fire tender would be able to enter from either direction on London Road North, turn within the site and exit in forward gear. On this basis, the layout of the site and design of the access is considered acceptable in terms of highway safety.
- 6.30 Taking account of the considerations and consultation responses discussed above, it is concluded that, subject to conditions and securing the sustainable travel measures discussed, the scheme provides an acceptable level of parking and would not give rise to adverse effects on highway safety or operation in the locality in terms of its access or servicing. It therefore complies with the requirements of policies Ho9, Mo5 and Mo7 of the Local Plan 2005 and the provisions of Policy CS17 of the Core Strategy.

Trees and landscaping

- 6.31 In its current state, the site is largely devoid of any tree cover and landscaping; however, there are a number of existing trees off-site along the adjoining highway bank/verge which contribute to the visual amenity of the area and the approach to the Conservation Area.
- 6.32 The application was supported by an arboricultural report which demonstrates that the development can be completed without the need to remove any trees. This has been reviewed by the Council's Tree Officer who agrees with this conclusion and further comments that there is unlikely to be any significant post development pressure placed on the trees (e.g. for removal or pruning which might degrade the visual amenity and appearance). On this basis, subject to conditions, the Tree Officer has raised no objection.
- 6.33 As above, the site is presently occupied either by buildings or hardstanding for car parking with no soft landscaping or planting giving a relatively harsh urban appearance. The scheme, whilst a relatively dense use of the site, would enable more meaningful landscaping to be introduced within the site including along the access road, within the rear parking court and in the communal amenity space. The detail of the landscaping will be secured through condition.

- 6.34 Accordingly subject to conditions requiring submission and implementation of a landscaping scheme and tree protection the proposal would not have an undue impact on the arboricultural interest of the site and has the potential to enhance the landscape character and visual amenity of the locality and would therefore comply with policies Pc4 and Ho9 of the Borough Local Plan 2005.

Community Infrastructure Levy (CIL) and requested contributions

- 6.35 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, roads, public transport and community facilities which are needed to support new development.
- 6.36 The proposal, being for a C3 residential use, falls within the uses which attract a charge based on the Council's adopted Charging Schedule and as such the development would be liable to pay CIL. The amount due would be formally determined in due course should permission be granted; however, based on the plans submitted the indicative charge would be in the region of £185,000 (prior to indexation and any relief claimed on the affordable housing units). In terms of other contributions and planning obligations, the Community Infrastructure Levy (CIL) Regulations which were introduced in April 2010 which states that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions, works or other obligations that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence.
- 6.37 In this case, the County Highway Authority has requested improvements to nearby bus stops in the village. Given the reduced parking provision proposed within the scheme, the above contributions are considered to meet the above tests and are necessary in terms of ensuring public transport options are genuine alternatives to private car use and ownership, particularly for shorter journeys such as the day-to-day shops, services and leisure facilities which are not available in Merstham Village but can be accessed in nearby Redhill for example. These improvements will be secured by condition. The provision of the car club measures proposed by the applicant in their Transport Statement will also be secured through a legal agreement.

Affordable housing

- 6.38 Core Strategy Policy CS15 and the Council's Affordable Housing SPD sets out that, on schemes of 15 or more net units such as this, the Council will expect 30% of units on-site to be provided as affordable housing.
- 6.39 The scheme includes provision for 10 affordable housing units on-site. The affordable housing units would be a mixture of 1 and 2 bedrooms which is broadly consistent with the market housing mix as required by the Council's SPD. Whilst all of the units are proposed as shared ownership tenure (rather than mixed tenure

advised through the SPD), this is justified by evidence from registered providers who have indicated that mixed tenure would not be attractive on this scheme due to the associated management and service charge complications for a relatively small number of units. A similar position was accepted on the Liquid and Envy scheme which provided 15 units. The affordable housing will be secured through a legal agreement.

- 6.40 The scheme therefore meets, in full, the requirements of Policy CS15 in respect of the amount of affordable housing and size mix and the approach to tenure is justified. The benefits of affordable housing provision would not be realised were the scheme to come forward for a conversion under prior approval.

Other matters

- 6.41 The proposal would make a positive contribution towards meeting the identified housing needs and requirements of the borough, with consequent local financial, economic and social benefits. There is no requirement for the applicant to demonstrate a specific need for this development, in this location. The development would make effective use of a previously developed (brownfield) site, consistent with national and local policies which prioritise the use of sustainable urban sites. Both of these are considered to add further, albeit modest, weight in favour of the proposal.
- 6.42 The site is partially within the A23 Merstham High Street Air Quality Management Area. Given this situation, the application was accompanied by an Air Quality Assessment. This has been reviewed by the Council's Environmental Health team who has confirmed that that assessment is acceptable and that air quality would meet legal standards. They therefore have no objection from an air quality perspective subject to securing the mitigation advised in the applicant's report (including provision of electric vehicle charging points and use of low NOx boilers). These measures are proposed to be secured through condition.
- 6.43 The site is in Flood Zone 1 according to Environment Agency Flood Maps and is therefore at low risk of river flooding. The application was supported by a Drainage Impact Assessment which, due to underlying geology, concludes that infiltration is unfeasible. On this basis, a system of permeable paving and storage with controlled discharge to the surface water sewer is proposed. Following dialogue and additional information from Thames Water (provided by the applicant) to confirm that there is sufficient capacity in the sewer network to accept the discharge from this site, Surrey CC SUDs Consenting Team have confirmed that they have no objection subject to conditions regarding the final design, implementation and maintenance of the system.
- 6.44 Whilst concerns were raised in respect of crime, no specific issues have been cited within the representations nor otherwise identified. The proposals are not considered to cause any particular crime risk than any other conventional residential development.

CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Floor Plan	1823_0151	A	08.11.2017
Floor Plan	1823_0152	A	08.11.2017
Floor Plan	1823_0150	A	08.11.2017
Elevation Plan	1823_0250	B	08.11.2017
Location Plan	1823_0001	B	01.11.2017
Site Layout Plan	1823_0020	G	27.03.2018
Site Layout Plan	1823_0030	C	27.03.2018
Floor Plan	1823_0109	V	27.03.2018
Floor Plan	1823_0110	Q	27.03.2018
Floor Plan	1823_0111	P	27.03.2018
Floor Plan	1823_0112	Q	27.03.2018
Roof Plan	1823_0113	N	27.03.2018
Elevation Plan	1823_0200	E	27.03.2018
Elevation Plan	1823_0201	E	27.03.2018
Section Plan	1823_0202	B	27.03.2018
Other Plan	161661-002	C	27.03.2018
Other Plan	161661-003	A	27.03.2018

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. No development shall commence including groundworks preparation and demolition until all related arboricultural matters, including arboricultural supervision, monitoring and tree protection measures are implemented in strict accordance with the approved details contained in the Tree Protection Plan and Arboricultural Method Statement compiled by SJA air 17063 -01a dated November 2017

Reason:

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan 2005 and the recommendations within British Standard 5837.

4. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) Parking for vehicles of site personnel, operatives and visitors
 - (b) Loading and unloading of plant and materials
 - (c) Storage of plant and materials
 - (d) Programme of works (including measures for traffic management)
 - (e) Provision of boundary hoarding behind any visibility zones
 - (f) Measures to prevent the deposit of materials on the highway
- Has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

5. No development shall commence on site until a scheme for the landscaping of the site including the retention of existing landscape features has been submitted and approved in writing by the Local Planning Authority. Landscaping schemes shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or use of the approved development or in accordance with a programme agreed in writing with the Local Planning Authority.

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837: Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan 2005.

6. No development shall commence until a written comprehensive Phase 1 environmental desktop study report is required to identify and evaluate possible on and off site contamination sources, pathways and receptors and enable the presentation of all plausible pollutant linkages in a preliminary conceptual site model. The study shall include any relevant regulatory consultations such as with the Contaminated Land Officer and be submitted to the Local Planning Authority and is subject to the approval in writing of the Local Planning Authority and any additional requirements that it may specify. The report shall be prepared in broad

accordance with the Environment Agency's Model Procedures for the Management of Contaminated Land (CLR11) and British Standard BS 10175.

Reason:

To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to policy CS10 of the Reigate and Banstead Core Strategy 2014 and the NPPF.

7. Should the Phase 1 study identify ground contamination which requires remediation, the following additional information, and any additional requirements that the Local Planning Authority may specify, should be submitted to and approved in writing by the Local Planning Authority prior to the commencement of development and prior to any activities specified:
- (a) A contaminated land site investigation proposal detailing the extent and methodologies of sampling, analyses and proposed assessment criteria to enable the characterisation of the plausible pollutant linkages identified in the preliminary conceptual model
 - (b) Prior to any site investigation work being commenced on site, a contaminated land site investigation and risk assessment undertaken in accordance with the above site investigation proposal as approved and reported in accordance the standards of DEFRA's and the Environment Agency's Model Procedures for the Management of Contaminated Land (CLR 11) and British Standard BS10175 which determines the nature and extent of contamination on the site
 - (c) If applicable, ground gas assessments completed in line with CIRIA C665 guidance
 - (d) Prior to any remediation being commenced on site, a detailed remediation method statement that explains the extent and method(s) by which the site is to be remediated and provides details of the information to be included in a validation report
 - (e) Contamination not previously identified by the site investigation, but subsequently found to be present at the site, shall be reported to the Local Planning Authority as soon as is practicable. If deemed necessary by the Local Planning Authority, development shall cease on site until an addendum to the remediation method statement detailing how the unsuspected contamination is to be dealt with, has been submitted in writing to the Local Planning Authority. The remediation method statement is subject to the written approval of the Local Planning Authority and any additional requirements that it may specify.

Following approval of the details in relation to parts (b) and (d) above, the Local Planning Authority shall be given a minimum of two weeks before the relevant investigation or remediation works commence on site.

Reason:

To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to Reigate and Banstead Borough Council Local Plan 2005 and the NPPF.

8. No development shall commence until an intrusive pre-demolition and refurbishment asbestos survey in accordance with HSG264 has been carried out and submitted to and approved in writing by the Local Planning Authority. The

survey shall be carried out by a suitably qualified person and shall include details of removal and mitigation.

Reason:

To ensure that a strategy is put in place for addressing contamination before development commences and to make the land/buildings suitable for development without resulting in risk to construction workers, future users, nearby occupiers and the environment with regard to policy CS10 of the Reigate and Banstead Core Strategy and the National Planning Policy Framework.

9. Notwithstanding the drawings, the development should be carried out using the external finishing materials and details specified below and there shall be no variation without the prior approval in writing of the Local Planning Authority:
- (a) All brickwork shall be of handmade sandfaced brick in Flemish bond, a sample to be submitted
 - (b) All tiles and tile hanging shall be of Wealden handmade sandfaced plain clay tiles, the hanging tile being lighter in the colour than the roof tiles, samples to be submitted
 - (c) All tile hanging to gables should be of straight lines with no Winchester cut
 - (d) All external joinery shall be of painted timber with architraved bargeboards with no box ends
 - (e) All windows shall be casement windows shall be of white painted timber with casements in each opening and external glazing bars of traditional profile
 - (f) All fascias shall be no more than two bricks depth
 - (g) All rainwater goods shall be of black finished cast metal or cast metal profile guttering and downpipes, written details to be submitted
 - (h) All rooflights shall be black painted metal conservation rooflight with a single vertical glazing bar, written details to be submitted
 - (i) All window arches shall be of gauged brick arches and the soldier brick arches shown on the approved shall be omitted
 - (j) All footpaths and drives shall be of fixed gravel

Where specified above, samples or written details of the materials to be used shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of any above ground or superstructure works on the building hereby approved. The development shall thereafter be carried out in strict accordance with the approved details.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9, Ho13, Pc10, Pc12 and Pc13.

10. No development shall take place until the detailed design of the surface water drainage scheme has been submitted to an approved in writing by the Local Planning Authority. Such details should include:
- a) A finalised design and strategy that follows the principles set out in the approved drainage strategy (Flood Risk Statement by Ardent Consulting dated October 2017 ref: 161661-05)
 - b) Evidence that the proposed solution will effectively manage the 1 in 30 & 1 in 100 (+CC%) allowance for climate change storm events during all stages of the development and in occupation. Associated discharge rates shall be provided

using a discharge rate as close as practical to the Greenfield run-off rates of 0.8 litres/second for 1 in 1 year, 2.2 litres/second for 1 in 30 year and 3.2 litres/second for a 1 in 100 year + climate change event unless otherwise agreed with the Local Planning Authority

- c) Detailed drawings to include: a finalised drainage layout detailing the location of SuDS elements, pipe diameters, levels, long or cross sections of each drainage element including details of flow restrictions and how they will be protected from blockage
- d) Details of how SuDS elements will be protected against ingress of debris and siltation and root damage
- e) Details of construction phasing, including how SuDS and any temporary drainage will be managed during the works including dealing with flows, silt, prevention of pollution and construction loading.
- f) A plan showing exceedance or system failure flows and directions, building finished floor levels, external finished levels and how property on and off site will be protected.
- g) Details of management and maintenance regimes and who will be responsible for the maintenance of the SuDS.

Reason:

To ensure that the development is served by an adequate and approved means of drainage which would not increase flood risk on or off site and is suitably maintained throughout its lifetime to comply with Policy Ut4 of the Reigate and Banstead Borough Local Plan 2005, Policy CS10 of the Core Strategy 2014 and the requirements of non-statutory technical standards.

11. The development hereby permitted shall be carried out in accordance with the approved Air Quality Assessment produced by Ardent Consulting dated October 2017, with particular regard to the recommendations in relation to:
- (a) mitigating construction activities (Table 6.1)
 - (b) the provision of at least 5 Electric Vehicle charge points which should have a minimum power of 7kW
 - (c) the use of low NOx emission boilers meeting a standard of <40mg NOx/kWh

Reason:

To ensure that the development would not give rise to unacceptable impacts on air quality or put future occupants at unacceptable risk of poor air quality with regard to policy Ho9 of the Reigate and Banstead Borough Local Plan 2005 and policy CS10 of the Reigate and Banstead Core Strategy.

12. The development hereby permitted shall be carried out in accordance with the approved Noise Impact Assessment produced by Ardent Consulting dated March 2018 (reference 161661-08A), with particular regard to the recommendations in relation to external building fabric (including ventilation and glazing) to prevent noise intrusion into residential units

Reason:

To ensure that the development would not give rise to unacceptable impacts on noise pollution or put future occupants at unacceptable risk of noise disturbance with regard to policies Ho9 and Ho10 of the Reigate and Banstead Borough Local Plan 2005 and policy CS10 of the Reigate and Banstead Core Strategy.

13. The development hereby approved shall not be first occupied until the space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking/turning areas shall be retained and maintained for their designated purpose.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

14. The development hereby approved shall not be first occupied unless and until improvements to the northbound and southbound bus stops on the A23 High Street have been completed in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority. Such improvements shall be confined to land within the ownership of either the Local Planning Authority or the County Highway Authority and shall be in broad accordance with those set out on pages 17 and 18 of the Transport Statement and the objectives of the Merstham Conservation Area Appraisal.

Reason:

To ensure that the development would promote sustainable transport choices with regard to Policy CS17 of the Reigate and Banstead Core Strategy 2014 and in recognition of Section 4 "Promoting Sustainable Transport" in the National Planning Policy Framework 2012

15. The residential units hereby approved shall not be first occupied unless and until a 'Residents Travel Pack' containing details of the availability of and whereabouts of local public transport (including up to date bus and travel information), walking and cycling routes, cycle storage, promotion of car sharing schemes and the nearest local amenities, leisure, health facilities and schools has been submitted to and approved in writing by the Local Planning Authority.

The Travel Packs shall thereafter be provided to each household within the development upon their taking occupation.

Reason:

To ensure that the development would promote sustainable transport choices with regard to Policy CS17 of the Reigate and Banstead Core Strategy 2014 and in recognition of Section 4 "Promoting Sustainable Transport" in the National Planning Policy Framework 2012

16. The development hereby approved shall not be first occupied until facilities for the secure, accessible storage of a minimum of 33 bicycles have been provided within the site in accordance with the approved plans. Thereafter, the bicycle storage facility shall be retained and maintained for its designated purpose.

Reason:

To ensure that the development would promote sustainable transport choices with regard to Policy CS17 of the Reigate and Banstead Core Strategy 2014 and in recognition of Section 4 "Promoting Sustainable Transport" in the National Planning Policy Framework 2012

17. The development hereby approved shall not be first occupied until details of external lighting within the site have been submitted to and approved in writing by the Local Planning Authority. The lighting shall be installed prior to occupation and thereafter maintained in accordance with the approved details.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Ho13.

18. The development hereby approved shall not be first occupied until refuse storage facilities have been provided in accordance with the approved plans. The said facilities shall thereafter be retained exclusively for its designated purpose.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Ho13.

19. The development hereby approved shall not be first occupied unless and until a verification report demonstrating that the sustainable urban drainage system has been constructed as per the agreed scheme has been submitted to and approved in writing by the Local Planning Authority. The validation report should be carried out by a qualified drainage engineer.

Reason:

To ensure that the development is served by an adequate and approved means of drainage which would not increase flood risk on or off site and is suitably maintained throughout its lifetime to comply with Policy Ut4 of the Reigate and Banstead Borough Local Plan 2005, Policy CS10 of the Reigate and Banstead Core Strategy 2014 and the requirements of non-statutory technical standards.

20. The development hereby approved shall not be first occupied unless and until a remediation validation report for the site shall be submitted to the Local Planning Authority in writing. The report shall detail evidence of the remediation, the effectiveness of the remediation carried out and the results of post remediation works, in accordance with the approved remediation method statement and any addenda thereto, so as to enable future interested parties, including regulators, to have a single record of the remediation undertaken at the site. Should specific ground gas mitigation measures be required to be incorporated into a development the testing and verification of such systems should be in accordance with CIRIA C735 guidance document entitled 'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases'.

Reason:

To demonstrate the effectiveness of remediation works and demonstrate that the proposed development will not cause harm to human health or pollution of controlled waters with regard to Reigate and Banstead Borough Council Local Plan 2005 Policy and the NPPF.

21. No plant or machinery, including fume extraction, ventilation and air conditioning, which may be required by reason of granting this permission, shall be installed within or on the building without the prior approval in writing of the Local Planning Authority. Any approved plant or machinery shall be installed and thereafter

maintained in accordance with the approved details and any manufacturer's recommendations.

Reason:

To ensure that a satisfactory external appearance is achieved of the development and to safeguard the amenities of neighbouring occupiers with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Ho13.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.
3. The applicant is advised that prior to the occupation of the development, adequate provision should be made for waste storage and collection. You are advised to contact the Council's Recycling and Cleansing team to discuss the required number and specification of wheeled bins on rc@reigate-banstead.gov.uk or on the Council's website at http://www.reigate-banstead.gov.uk/info/20051/commercial_waste.
4. Your attention is drawn to the benefits of using the Secured by Design award scheme.
5. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit. In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is

registered with the Considerate Constructors Scheme -
www.ccscheme.org.uk/index.php/site-registration.

6. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
7. The use of a suitably qualified arboricultural consultant is essential to provide acceptable supervision and monitoring in respect of the arboricultural issues in respect of the above condition. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
8. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. Replacement planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity and long term continued structural tree cover in this area, including along the Albury Road frontage and any landscaping submission will be expected to reflect this. It is expected that the replacement structural landscape trees will be of Advanced Nursery Stock sizes with initial planting heights of not less than 4.5m with girth measurements at 1m above ground level in excess of 16/18cm as a minimum.
9. The applicant's attention is drawn to the specific requirements of the contaminated land conditions, particularly in respect of the timing of submissions and requirements for prior notice to be given before commencement of site investigations and/or remediation. The submission of information not in accordance with the specific timing requirements can lead to delays in discharging conditions, potentially result in conditions being unable to be discharged or even enforcement action should the required level of evidence/information be unable to be supplied.
10. With respect to the bus stop improvements required by Condition 14, whilst regard should be had to the scope of works and type of improvements proposed on pages 17 and 18 of the submitted Transport Statement (by Ardent Consulting), the final design and details of any improvements will need to be sympathetic to the character and appearance of the Merstham Village Conservation Area and protection and enhancement of the village green where the northbound shelter is located. For example, full bus boarders and extensive new footways are unlikely to be appropriate for the northbound bus stop. You are advised to seek advice from the Borough Council (and in particular the Conservation Officer) on any emerging proposals prior to submission for discharging the condition.

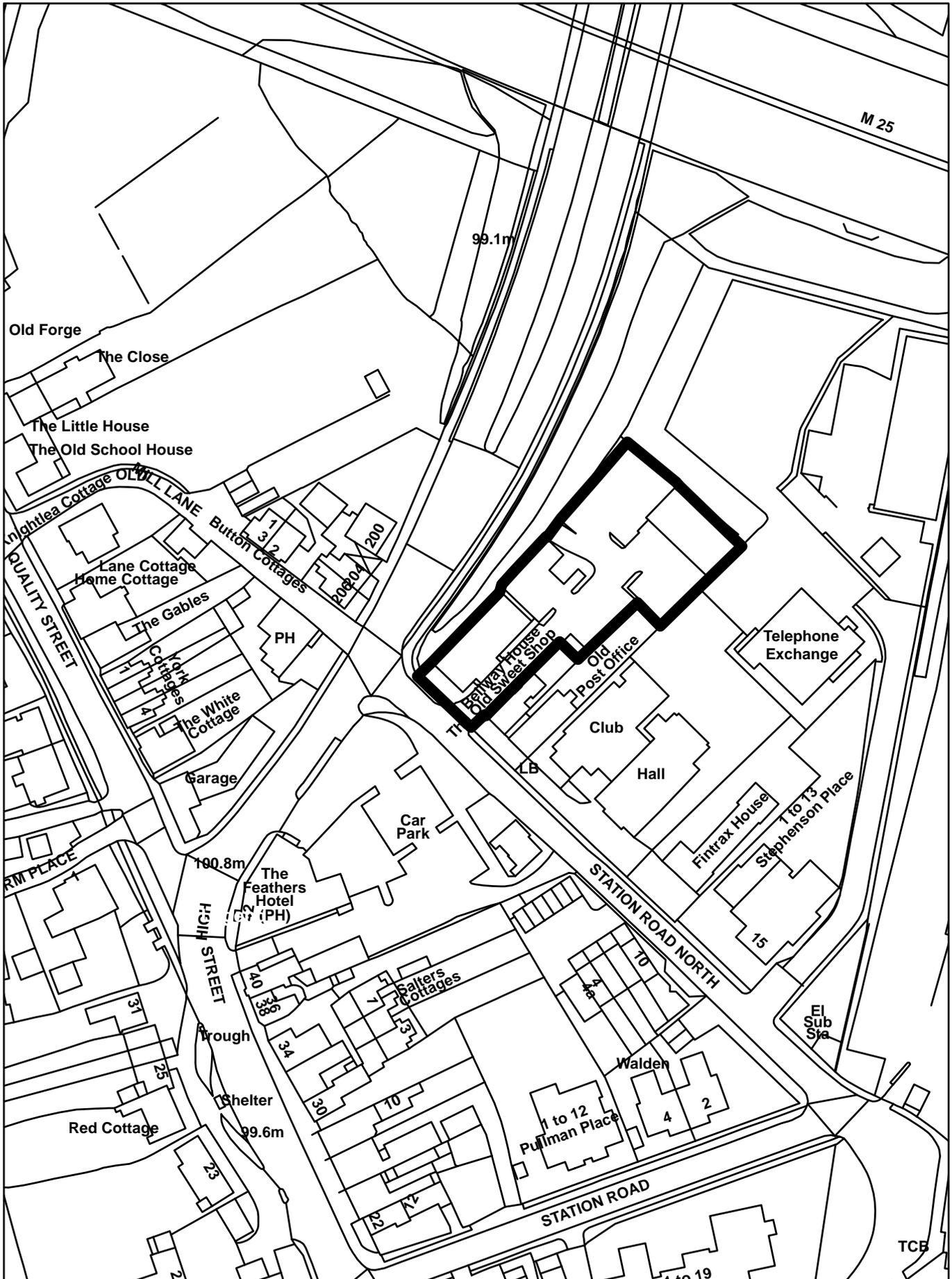
REASON FOR PERMISSION

The development hereby permitted has been assessed against development plan policies CS1, CS4, CS5, CS10, CS11, CS12, CS13, CS14, CS15, CS17, Pc2G, Pc8, Pc9, Pc10, Pc13, Ho9, Ho10, Ho13, Ho16, Em1A, Mo4, Mo5, Mo6, Mo7 and Ut4 and material considerations, including third party representations. It has been concluded that the

development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

17/02542/F - Bellway House, 241 London Road North, Merstham





Notes:

- Do not scale from this drawing; work to figured dimensions only.
- All information is to be checked on site for accuracy and fit.
- Report any discrepancies and omissions to RMA Architects.

H&S Notes - "ERIC" (Eliminate, Reduce, Isolate, Control)



- Where RMA services on a project do not include for site inspections and works surveys, RMA do not warrant that 'as built' issue drawings are a complete and accurate record of what has been constructed.
- Unless notified specifically by the responsible party, changes made on site to RMA design information has not been incorporated in this drawing.

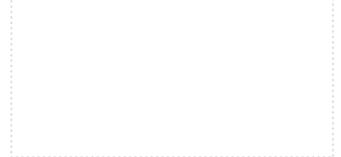
Diagram Key



Rev	Date	Description	By	Chkd
C	23.03.18	RE-ISSUED FOR TOWN PLANNING	KH	AM
B	20.03.18	Block D removed	KH	AM
A	31.10.17	ISSUED FOR TOWN PLANNING	LL	AM
-	17.10.17	Design fix for consultants	LL	AM

RMA Architects
 3 Ella Mews
 Cressy Road
 London
 NW43 2NH
 t 020 7284 1414
 www.rmaarchitects.co.uk
 © RMA

Key Plan



Project
Bellway House
 Station Road North
 Merstham RHI 3YU

Client
Bellway Homes
 (South London) and
 SAL Pension Fund Ltd

Scale	Date	By	Checked
1:500	October 2017	LL	AM

Drawing Title
Proposed Site Plan
 with combined
 Ground Floor Levels

Drawing Status	Drawing No	Revision
TOWN PLANNING	1823_0030	C

SPECIFICATION
All works generally, to comply with the written Soft Landscape Specification.
NOTE: DO NOT SCALE FROM DRAWING

Legend

-  Existing trees and hedges to be retained and protected during construction
-  Proposed tree planting within soft landscape. See detail on sheet
-  Proposed specimen shrub planting to receive 75mm bark mulch after planting operations
-  Proposed climbing plants to be trained up adjacent wall/ fence/pergola with galvanised wires and hook set 250mm apart with support canes removed prior to planting
-  Proposed *Prunus lusitania* (portuguese laurel) hedge planting to be planted in a double staggered row 300mm apart and at 500mm centres in each row. To be maintained at 1.2m height
-  Proposed shrub planting to receive 75mm bark mulch after planting operations
-  Proposed decorative mix shrub planting to be planted in groups of 5-7no. of each species and as above
-  Proposed grass areas to receive good quality amenity grass turves laid in line with good horticultural practices
-  Proposed bitmac surface, installed in line with engineer's specification
-  Proposed 'Original Tegula' block paving, 120mm x 160mm, 120mm x 120mm and 120mm x 80mm in pennant grey colour. Available from Marshalls or similar. Installed in line with engineer's specification
-  Proposed 'Perfecta Smooth Ground' flag paving, 600mm x 600mm in natural colour. Available from Marshalls or similar. Installed in line with engineer's specification
-  Proposed 'Original Tegula' block paving, 120mm x 160mm, 120mm x 120mm and 120mm x 80mm in harvest colour. Available from Marshalls or similar. Installed in line with engineer's specification
-  Proposed locally sourced, decorative gravel to be laid to a minimum depth of 50mm on a weed proof membrane.
-  Proposed close board timber fence, 1.8m high.



Rev	Date	Details	Drawn

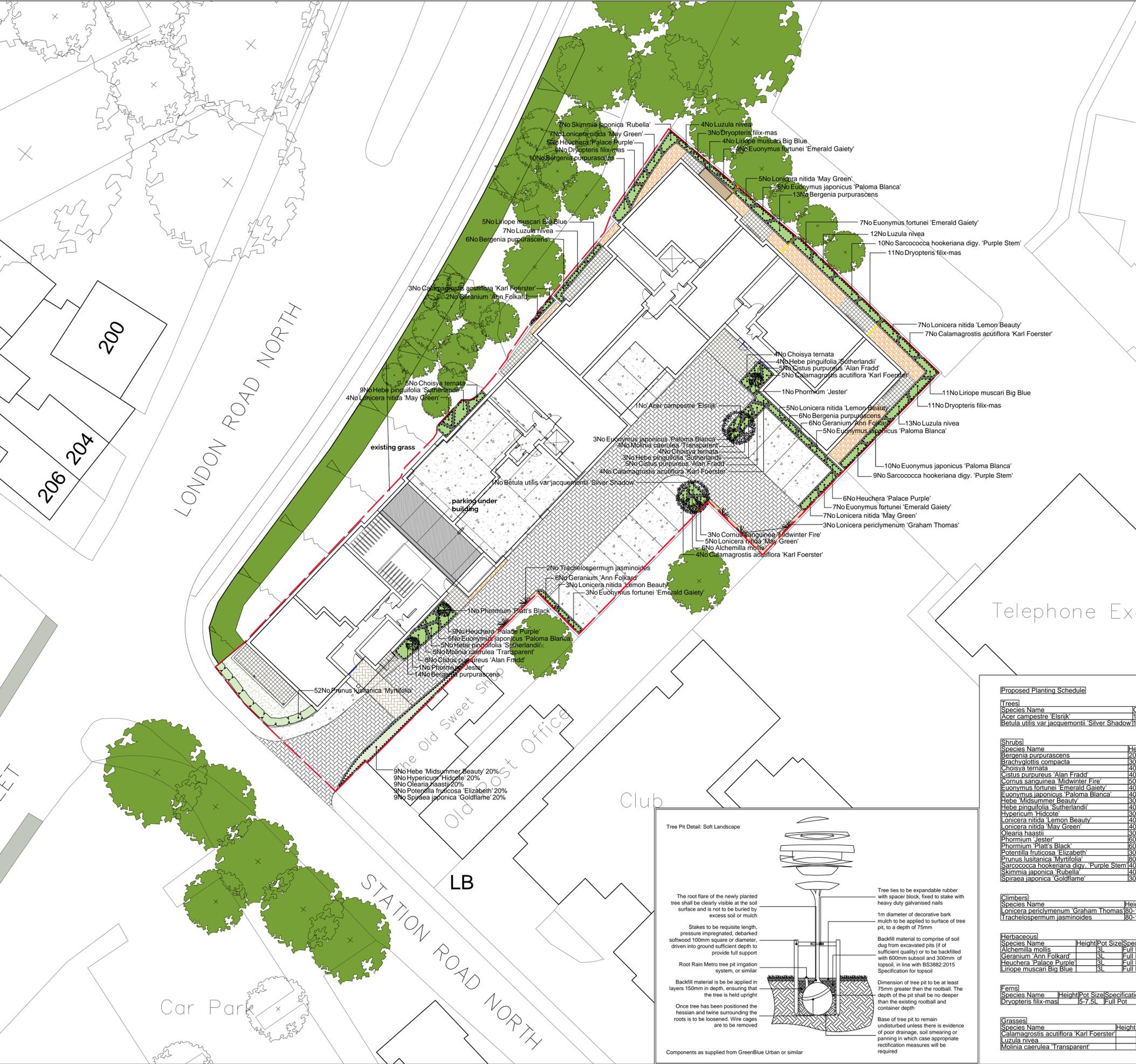


ACD ENVIRONMENTAL
HEAD OFFICE
Rodbourn Rail Business Centre, Grange Lane,
Malmesbury, SN16 0ES
Tel: 01666 825646
Courtyard House, Mill Lane, Godalming, GU7 1EY
Tel: 01483 425714
Suite 6, Crescent House, Yonge Close, Eastleigh, SO50 9SX
Tel: 02382 026300
9 Brownlow Road, Cambridge, CB4 3NG
Tel: 07825 868654
email: mail@acdenv.co.uk
www.acdenvironmental.co.uk



scheme: Bellway House, Station Road North, Merstham
client: Bellway Homes (South London) Ltd
SAL Pension Fund Ltd
drawing: Landscape Proposals
date: Oct 2017
scale: 1:200@A1
drawing no: BELL21481-11
drawn: DPM checked: ALK

Copyright of ACD Environmental Ltd. All rights described in Chapter IV of the Copyright, Designs and Patents Act 1988 have been generally asserted. 2011. Copyright of this plan remains with ACD until all fees have been paid in full.



Proposed Planting Schedule

Trees	Species Name	Girth	Height	Density	Specification	No.
	Acer campestre 'Elsrijk'	12-14cm	350-425cm	Counted	Heavy Standard, Clear Stem 175-200, 5 brks, RBf	1No
	Betula utilis var jacquemontii 'Silver Shadow'	12-14cm	350-425cm	Counted	Heavy Standard, Clear Stem 175-200, 5 brks, RBf	1No

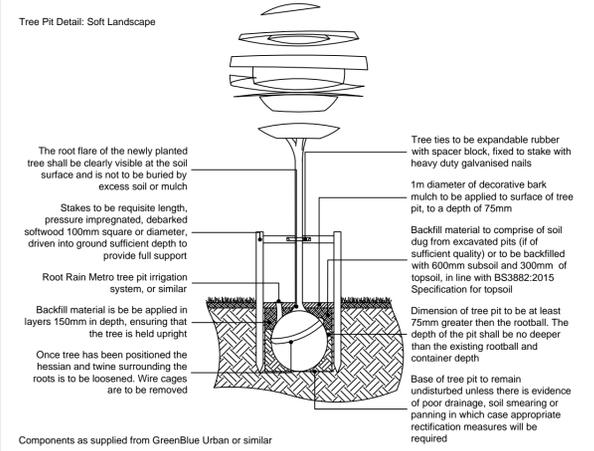
Shrubs	Species Name	Height	Pot Size	Specification	Density	No.
	Bergenia purpurascens	20-30cm	3L	Bushy C	4/m ²	49No
	Brachyotus compacta	30-40cm	5L	Bushy 5 brks C	3/m ²	4No
	Choisya ternata	40-60cm	5L	Bushy C	3/m ²	13No
	Cistus purpureus 'Alan Fradd'	40-60cm	5L	Bushy C	3/m ²	18No
	Cornus sanguinea 'Midwinter Fire'	50-60cm	5L	Branched C	3/m ²	3No
	Euonymus fortunei 'Emerald Gaiety'	40-60cm	5L	Bushy C	3/m ²	21No
	Euonymus japonicus 'Paloma Blanca'	40-60cm	5L	Bushy C	3/m ²	29No
	Hebe 'Midsummer Beauty'	30-40cm	3L	Bushy C	3/m ²	9No
	Hebe pinguifolia 'Sutherlandii'	40-60cm	5L	Bushy C	3/m ²	21No
	Hypericum 'Hidcote'	30-40cm	3L	Bushy C	3/m ²	9No
	Lonicera nitida 'Lemon Beauty'	40-60cm	5L	Bushy C	3/m ²	15No
	Lonicera nitida 'May Green'	40-60cm	5L	Bushy C	3/m ²	28No
	Pearia naastii	30-40cm	3L	Bushy C	3/m ²	9No
	Phormium 'Jester'	60-90cm	15L	7 leaves C	Counted	2No
	Phormium 'Platt's Black'	60-90cm	15L	7 leaves C	Counted	1No
	Potentilla fruticosa 'Elizabeth'	30-40cm	3L	Bushy C	3/m ²	9No
	Prunus lusitania 'Myrtifolia'	80-100cm	10L	Bushy 5/6 brks C, 5Ctr Double Staggered at 0.3m	5/2No	18No
	Sarcococca hookeriana digy. 'Purple Stem'	40-60cm	5L	Bushy 5/6 brks C	3/m ²	7No
	Skimmia japonica 'Rubella'	40-60cm	5L	Bushy 5/6 brks C	3/m ²	7No
	Spiraea japonica 'Goldflame'	30-40cm	3L	Bushy C	3/m ²	9No

Climbers	Species Name	Height	Pot Size	Specification	Density	No.
	Lonicera periclymenum 'Graham Thomas'	80-100cm	10L	Several Shoots 3/5 brks, Caned	Counted	3No
	Trachelospermum jasminoides	80-100cm	10L	Several Shoots 3/5 brks, Caned	Counted	2No

Herbaceous	Species Name	Height	Pot Size	Specification	Density	No.
	Alchemilla mollis	9L	Full Pot	4/m ²	6No	6No
	Geranium 'Ann Folkard'	9L	Full Pot	4/m ²	14No	14No
	Heuchera 'Palace Purple'	9L	Full Pot	4/m ²	20No	20No
	Liriope muscari 'Big Blue'	9L	Full Pot	4/m ²	20No	20No

Ferns	Species Name	Height	Pot Size	Specification	Density	No.
	Dryopteris filix-mas	5-7.5L	Full Pot	4/m ²	13/4No	13/4No

Grasses	Species Name	Height	Pot Size	Specification	Density	No.
	Calamagrostis acutiflora 'Karl Foerster'	5L	Full Pot	3/m ²	23No	23No
	Luzula nivea	5L	Full Pot	4/m ²	36No	36No
	Molinia caerulea 'Transparent'	5L	Full Pot	3/m ²	13No	13No



KEY
 — Pavement Groundline
 — Existing Groundline

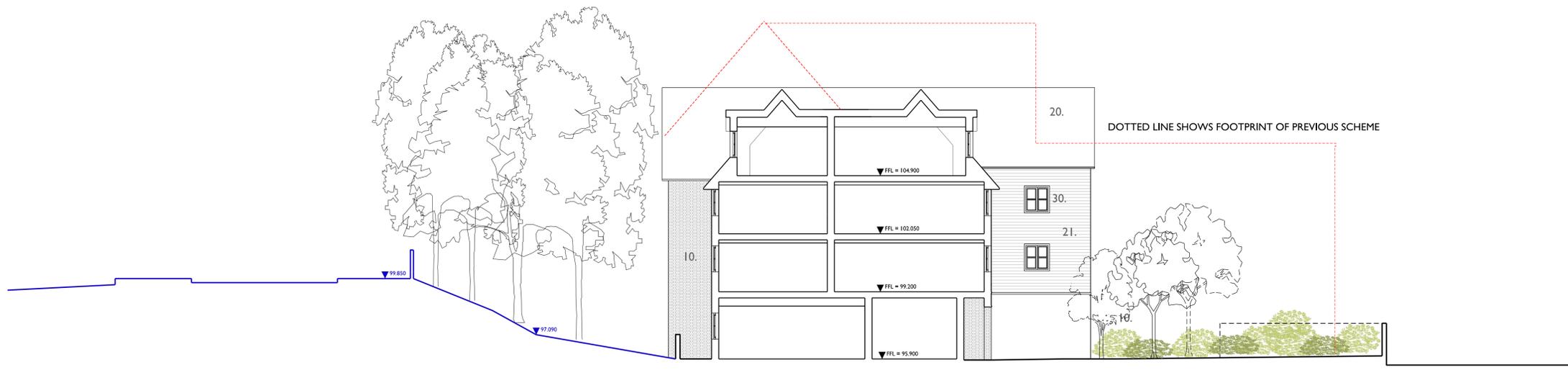
Notes:
 • Do not scale from this drawing; work to figured dimensions only.
 • All information is to be checked on site for accuracy and fit.
 • Report any discrepancies and omissions to RMA Architects.

H&S Notes - "ERIC" (Eliminate, Reduce, Isolate, Control)

• Where RMA services on a project do not include for site inspections and works surveys, RMA do not warrant that 'as built' issue drawings are a complete and accurate record of what has been constructed.
 • Unless notified specifically by the responsible party, changes made on site to RMA design information has not been incorporated in this drawing.

SCALE BAR: 1:125 @A1, 1:250 @A3
 0 2 4 6 8 10m

Diagram Key



Cross Section and South West Elevation within site

Rev	Date	Description	By	Chkd
B	23.03.18	RE-ISSUED FOR TOWN PLANNING	KH	AM
A	31.10.17	ISSUED FOR TOWN PLANNING	LL	AM
-	27.10.17	Issued for Comment	LL	AM

RMA ARCHITECTS
 RMA Architects
 3 Ella Mews
 Cressy Road
 London
 NW3 2NH
 t 020 7284 1414
 www.rmaarchitects.co.uk
 © RMA

Key Plan

Project
Bellway House
Station Road North
Merstham RHI 3YU

Client
Bellway Homes
(South London) and
SAL Pension Fund Ltd

Scale	Date	By	Checked
1:125 @A1	May 2017	LL	AM
1:250 @A3			

Drawing Title
Proposed Cross Section
and South West Elevation
within site

Drawing Status	Revision
PLANNING	
Drawing No 1823_0202	Revision B

- Materials Key**
- 10. Red stock brick
 - 11. Soldier course to window head
 - 12. Brick on edge sub sill
 - 13. Brick banding detail
 - 14. Projecting quoin detail
 - 15. Corbel detail
 - 16. Bricked window opening
 - 20. Plain tile roof
 - 21. Patterned tile hanging
 - 22. Dark stained timber boarding
 - 23. Cream coloured render
 - 30. White small pane windows and doors
 - 31. White bargeboards, fascias and soffits
 - 32. Black rainwater goods
 - 33. Conservation roof light
 - 40. Black and white painted balcony
 - 41. Black and white painted Juliet balustrade

PROPOSED NORTH
WEST ELEVATION
(VIEWED FROM
LONDON ROAD)



- Notes:
- Do not scale from this drawing; work to figured dimensions only.
 - All information is to be checked on site for accuracy and fit.
 - Report any discrepancies and omissions to RMA Architects.

H&S Notes - "ERIC" (Eliminate, Reduce, Isolate, Control)

- Where RMA services on a project do not include for site inspections and works surveys, RMA do not warrant that 'as built' issue drawings are a complete and accurate record of what has been constructed.
- Unless notified specifically by the responsible party, changes made on site to RMA design information has not been incorporated in this drawing.



Diagram Key



South East Elevation



North East Elevation

Materials Key

- 10. Red stock brick
- 11. Soldier course to window head
- 12. Brick on edge sub sill
- 13. Brick banding detail
- 14. Projecting quoin detail
- 15. Corbel detail
- 16. Bricked window opening

- 20. Plain tile roof
- 21. Patterned tile hanging
- 22. Dark stained timber boarding
- 23. Cream coloured render

- 30. White small pane windows and doors
- 31. White bargeboards, fascias and soffits
- 32. Black rainwater goods
- 33. Conservation roof light

- 40. Black and white painted balcony
- 41. Black and white painted Juliet balustrade

Rev	Date	Description	By	Chkd
E	23.03.18	RE-ISSUED FOR TOWN PLANNING	KH	AM
D	14.02.18	Block D removed	KH	AM
C	30.01.18	- Block C roofline lowered + Block C south east wall pulled back	KH	AM
B	31.10.17	ISSUED FOR TOWN PLANNING	LL	AM
A	27.10.17	Issued for Comment	LL	AM



RMA Architects
3 Ella Mews
Cressy Road
London
NW3 2NH
t 020 7284 1414
www.rmaarchitects.co.uk
© RMA



Project
**Bellway House
Station Road North
Merstham RH1 3YU**

Client
**Bellway Homes
(South London) and
SAL Pension Fund Ltd**

Scale	Date	By	Checked
1:125 @A1	May 2017	LL	AM
1:250 @A3			

Drawing Title
**Proposed
North East & South East
Elevations**

Drawing Status
PLANNING

Drawing No	Revision
1823_0201	E

Notes:

- Do not scale from this drawing; work to figured dimensions only.
- All information is to be checked on site for accuracy and fit.
- Report any discrepancies and omissions to RMA Architects.

H&S Notes - "ERIC" (Eliminate, Reduce, Isolate, Control)

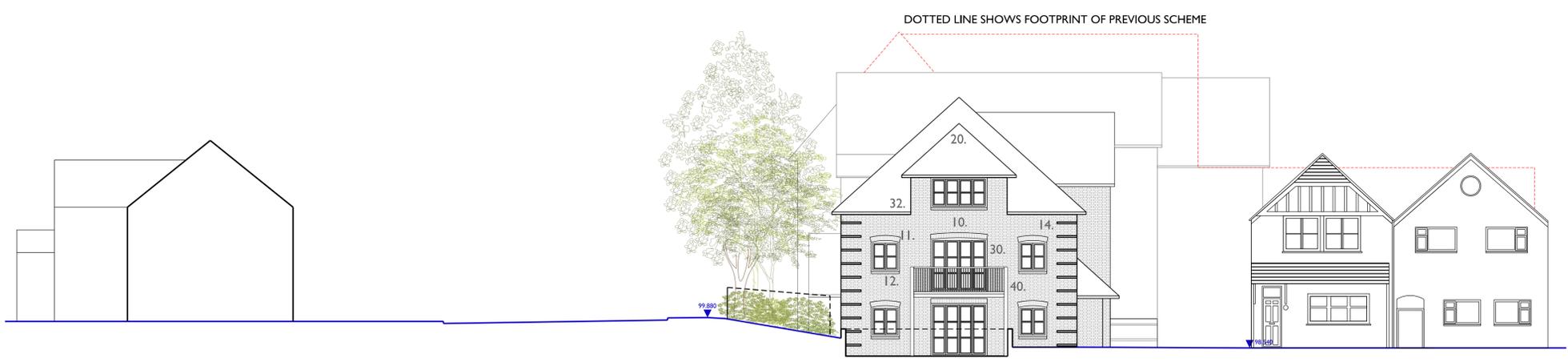
- Where RMA services on a project do not include for site inspections and works surveys, RMA do not warrant that 'as built' issue drawings are a complete and accurate record of what has been constructed.
- Unless notified specifically by the responsible party, changes made on site to RMA design information has not been incorporated in this drawing.

SCALE BAR: 1:125 @A1, 1:250 @A3

Diagram Key



North West Elevation



South West Elevation

KEY

- London Road Pavement Groundline
- Existing Groundline

Materials Key

- 10. Red stock brick
- 11. Soldier course to window head
- 12. Brick on edge sub sill
- 13. Brick banding detail
- 14. Projecting quoin detail
- 15. Corbel detail
- 16. Bricked window opening
- 20. Plain tile roof
- 21. Patterned tile hanging
- 22. Dark stained timber boarding
- 23. Cream coloured render
- 30. White small pane windows and doors
- 31. White bargeboards, fascias and soffits
- 32. Black rainwater goods
- 33. Conservation roof light
- 40. Black and white painted balcony
- 41. Black and white painted Juliet balustrade

Rev	Date	Description	By	Chkd
E	23.03.18	RE-ISSUED FOR TOWN PLANNING	KH	AM
D	14.02.18	Block D removed	KH	AM
C	30.01.18	Block C roofline lowered	KH	AM
B	31.10.17	ISSUED FOR TOWN PLANNING	LL	AM
A	27.10.17	Issued for Comment	LL	AM
-	19.10.17	First issued	LL	AM

RMA Architects
 3 Ella Mews
 Cressy Road
 London
 NW3 2NH
 t 020 7284 1414
 www.rmaarchitects.co.uk
 © RMA

Key Plan

Project
**Bellway House
 Station Road North
 Merstham RHI 3YU**

Client
**Bellway Homes
 (South London) and
 SAL Pension Fund Ltd**

Scale	Date	By	Checked
1:125 @A1	May 2017	LL	AM
1:250 @A3			

Drawing Title
**Proposed
 North West & South West
 Elevations**

Drawing Status	Drawing No	Revision
PLANNING	1823_0200	E

LONDON ROAD NORTH



Notes:
 • Do not scale from this drawing; work to figured dimensions only.
 • All information is to be checked on site for accuracy and fit.
 • Report any discrepancies and omissions to RMA Architects.

H&S Notes - "ERIC" (Eliminate, Reduce, Isolate, Control)



• Where RMA services on a project do not include for site inspections and works surveys, RMA do not warrant that 'as built' issue drawings are a complete and accurate record of what has been constructed.
 • Unless notified specifically by the responsible party, changes made on site to RMA design information has not been incorporated in this drawing.

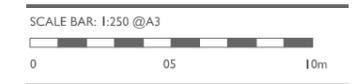
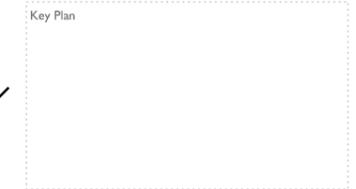


Diagram Key

Rev	Date	Description	By	Chkd
Q	23.03.18	RE-ISSUED FOR TOWN PLANNING	KH	AM
P	12.02.18	Step back amended to 903mm	KH	AM
N	08.02.18	Block D removed	KH	AM
M	31.10.17	ISSUED FOR TOWN PLANNING	LL	AM
L	17.10.17	Design for consultants	LL	AM
K	09.10.17	Updated to Client Comments	KH	AM
J	05.10.17	Updated to Client Comments	KH	AM
H	28.09.17	Flat units updated	LL	AM
G	27.09.17	Flat units and windows added	LL	AM
F	08.05.17	Amenity Space note added	LL	AM
E	05.05.17	Design Development	LL	AM
D	27.04.17	Design Development	LL	AM
C	12.04.17	Core D - Lift added	LL	CT
B	07.04.17	Re-Planned following Pre App Meeting	AM	AM



RMA Architects
 3 Ella Mews
 Cressy Road
 London
 NW3 2NH
 t 020 7284 1414
 www.rmaarchitects.co.uk
 © RMA



Project
**Bellway House
 Station Road North
 Merstham RHI 3YU**

Client
**Bellway Homes
 (South London) and
 SAL Pension Fund Ltd**

Scale	Date	By	Checked
1:250	March 2017	GS	AM

Drawing Title
**Proposed
 Upper Ground Floor Plan
 Street Level**

Drawing Status
TOWN PLANNING

Drawing No	Revision
1823_0110	Q



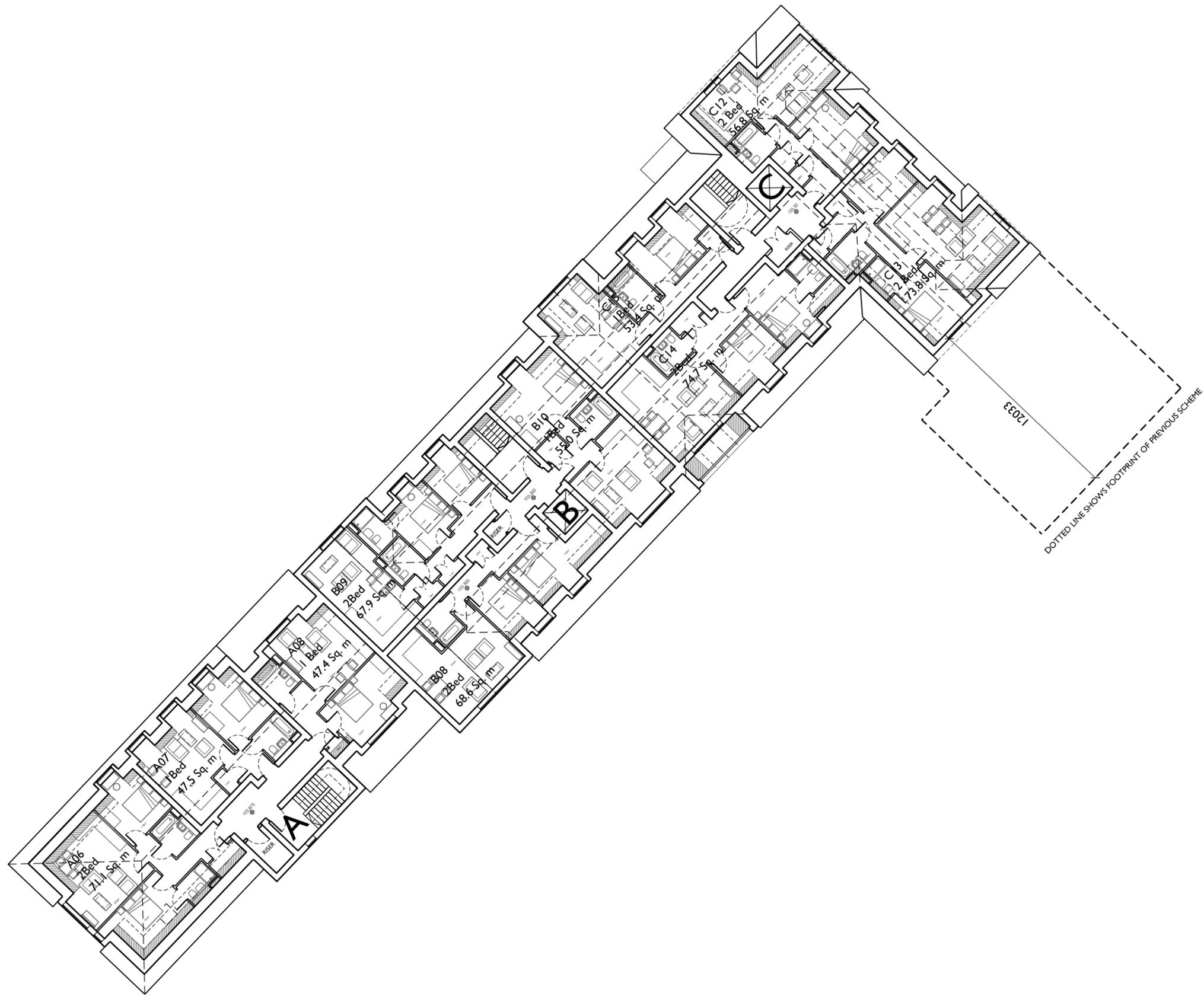
- Notes:
- Do not scale from this drawing; work to figured dimensions only.
 - All information is to be checked on site for accuracy and fit.
 - Report any discrepancies and omissions to RMA Architects.

H&S Notes - "ERIC" (Eliminate, Reduce, Isolate, Control)

- Where RMA services on a project do not include for site inspections and works surveys, RMA do not warrant that 'as built' issue drawings are a complete and accurate record of what has been constructed.
- Unless notified specifically by the responsible party, changes made on site to RMA design information has not been incorporated in this drawing.



Diagram Key



Q	23.03.18	RE-ISSUED FOR TOWN PLANNING	KH	AM
P	12.02.18	Step back amended to 9033mm	KH	AM
N	08.02.18	Block D removed	KH	AM
M	31.10.17	ISSUED FOR TOWN PLANNING	LL	AM
L	17.10.17	Design fit for consultants	LL	AM
K	09.10.17	Updated to Client Comments	KH	AM
J	05.10.17	• 1.9m head height area hatched • Bedrooms re-arranged to allow 600mm around beds.	KH	AM
H	28.09.17	Flat units updated	LL	AM
G	24.05.17	Unit Type amended to A06 & B09	LL	AM
F	22.05.17	Blocks B & C amended to dropped roof at 49° pitch	LL	AM
E	08.05.17	Amenity Space note added	LL	AM
D	05.05.17	Design Development	LL	AM
C	27.04.17	Design Development	LL	AM
B	17.04.17	Core D - Lift added	LL	CT
A	07.04.17	Re-Planned following Pre App Meeting	AM	AM

Rev	Date	Description	By	Chkd
-----	------	-------------	----	------



RMA Architects
3 Ella Mews
Cressy Road
London
NW3 2NH
t 020 7284 1414
www.rmaarchitects.co.uk
© RMA



Project
Bellway House
Station Road North
Merstham RHI 3YU

Client
Bellway Homes
(South London) and
SAL Pension Fund Ltd

Scale	Date	By	Checked
1:250	March 2017	GS	AM

Drawing Title
Proposed
Second Floor Plan

Drawing Status TOWN PLANNING	
Drawing No 1823_0112	Revision Q

LONDON ROAD NORTH



Notes:
 • Do not scale from this drawing; work to figured dimensions only.
 • All information is to be checked on site for accuracy and fit.
 • Report any discrepancies and omissions to RMA Architects.

H&S Notes - "ERIC" (Eliminate, Reduce, Isolate, Control)



• Where RMA services on a project do not include for site inspections and works surveys, RMA do not warrant that 'as built' issue drawings are a complete and accurate record of what has been constructed.
 • Unless notified specifically by the responsible party, changes made on site to RMA design information has not been incorporated in this drawing.

SCALE BAR: 1:250 @A3



Diagram Key

Rev	Date	Description	By	Chkd
V	23.03.18	RE-ISSUED FOR TOWN PLANNING	KH	AM
U	19.02.18	Unit areas increased	KH	AM
T	12.02.18	Step back amended to 903mm	KH	AM
S	08.02.18	Block D removed	KH	AM
R	31.10.17	ISSUED FOR TOWN PLANNING	LL	AM
P	17.10.17	Design fix for consultants	LL	AM
M	9.10.17	Updated to Client Comments	KH	AM
L	5.10.17	Updated to Client Comments	KH	AM
K	28.09.17	Flat units updated	LL	AM
J	27.09.17	Flat units added	LL	AM
H	08.05.17	Amenity Space note added	LL	AM
G	05.05.17	Design Development	LL	AM
F	27.04.17	Design Development	LL	AM
E	12.04.17	Core D - Lift added	LL	CT
D	07.04.17	Re-Planned following Pre App Meeting	AM	AM

RMA Architects
 3 Ella Mews
 Cressy Road
 London
 NW3 2NH
 t 020 7284 1414
 www.rmaarchitects.co.uk
 © RMA



Project

Bellway House
 Station Road North
 Merstham RHI 3YU

Client

Bellway Homes
 (South London) and
 SAL Pension Fund Ltd

Scale	Date	By	Checked
1:250	March 2017	GS	AM

Drawing Title

Proposed
Lower Ground Floor Plan

Drawing Status

TOWN PLANNING

Drawing No	Revision
1823_0109	V

The Old Sweet Shop
 Post Office

COMMUNAL AMENITY SPACE 12033
 2560
 DOTTED LINE SHOWS FOOTPRINT OF PREVIOUS SCHEME





- Notes:
- Do not scale from this drawing; work to figured dimensions only.
 - All information is to be checked on site for accuracy and fit.
 - Report any discrepancies and omissions to RMA Architects.

H&S Notes - "ERIC" (Eliminate, Reduce, Isolate, Control)



- Where RMA services on a project do not include for site inspections and works surveys, RMA do not warrant that 'as built' issue drawings are a complete and accurate record of what has been constructed.
- Unless notified specifically by the responsible party, changes made on site to RMA design information has not been incorporated in this drawing.



Diagram Key



Rev	Date	Description	By	Chkd
P	23.03.18	RE-ISSUED FOR TOWN PLANNING	KH	AM
N	12.02.18	Step back amended to 9033mm	KH	AM
M	08.02.18	Block D removed	KH	AM
L	31.10.17	ISSUED FOR TOWN PLANNING	LL	AM
K	17.10.17	Design fix for consultants	LL	AM
J	09.10.17	Updated to Client Comments	KH	AM
H	05.10.17	Updated to Client Comments	KH	AM
G	28.09.17	Flat units updated	LL	AM
F	27.09.17	Flat units added	LL	AM
E	08.05.17	Amenity Space note added	LL	AM
D	05.05.17	Design Development	LL	AM
C	27.04.17	Design Development	LL	AM
B	12.04.17	Core D - Lift added	LL	CT
A	07.04.17	Re-Planned following Pre App Meeting	AM	AM

RMA Architects
3 Ella Mews
Cressy Road
London
NW3 2NH
t 020 7284 1414
www.rmaarchitects.co.uk
© RMA



Project
**Bellway House
Station Road North
Merstham RHI 3YU**

Client
**Bellway Homes
(South London) and
SAL Pension Fund Ltd**

Scale	Date	By	Checked
1:250	March 2017	GS	AM

Drawing Title
**Proposed
First Floor Plan**

Drawing Status	Drawing No	Revision
TOWN PLANNING	1823_0111	P



