 <b>Reigate &amp; Banstead</b> <b>BOROUGH COUNCIL</b> Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	20 <sup>th</sup> January 2021
	<b>REPORT OF:</b>	HEAD OF PLACES & PLANNING
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<b>AGENDA ITEM:</b>	6	<b>WARD:</b> Earlswood and White Bushes

<b>APPLICATION NUMBER:</b>	20/01212/F	<b>VALID:</b>	15/06/2020
<b>APPLICANT:</b>	Beaufort Homes Ltd	<b>AGENT:</b>	Ka Architectural Ltd
<b>LOCATION:</b>	<b>29 WOODLANDS ROAD REDHILL SURREY RH1 6EY</b>		
<b>DESCRIPTION:</b>	<b>Construction of three terraced four bedroom houses including access road and parking/ landscaping. As amended on 20/07/2020, 27/10/2020 and 02/12/2020</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

This is a full application for the erection of three terraced four bedroom dwellings to the rear of 29 Woodlands Road in Redhill. The area is predominantly residential in character, defined by a mix of detached, semi-detached and terraced dwellings, alongside a smaller number of flatted developments.

The proposed dwellings would be of a traditional design, being hipped roofs with pitched roof gables to the front and rear elevations. The properties would utilise traditional materials, being tile hung to the principal elevation at the first floor with facing brickwork and modest elements of architectural detailing, the colour palette and design reflecting the character of the surrounding area and the Surrey Vernacular. The building would be parallel to the donor property and the road, with appropriately sized rear gardens and plots that would not be overly cramped.

The dwellings would provide four bedrooms and a level of living space that would comply with Policies DES5 of the Development Management Plan and Nationally Described Space Standards. The development would also accord with the Councils' Residential Parking Standards, requiring 6 parking spaces to be provided. Two further spaces would be retained to the front of the plot to provide parking for the donor property. The County Highway Authority has assessed the application and is satisfied that the development would not give rise to harm to the safe operation of the highway and that the level of parking is acceptable.

There are no protected trees on site however it is proposed to remove a number of trees to accommodate the development. These are of low quality and would be replaced as part of a future landscaping scheme. The site does have the potential to provide habitat for a variety of wildlife species; however protected species, such as bats, have not been identified as having a significant presence within the site. The Surrey Wildlife Trust has been consulted and have recommended that works proceed in a precautionary manner to ensure that existing habitats are not harmed or disturbed.

## **RECOMMENDATION(S)**

Planning permission is **GRANTED** subject to conditions.

## Consultations:

Highway Authority: has undertaken an assessment in terms of safety, capacity and policy grounds and have recommended conditions requiring the construction of the proposed access road in accordance with the approved plans, the provision of pedestrian inter-visibility splays, the provision of 9 parking spaces in accordance with the approved plans, the submission of a construction transport management plan prior to commencement of development, and the installation of electric vehicle charging points

Surrey Wildlife Trust: Initial comments were made requesting the submission of preliminary ecology surveys to establish the ecological potential of the site and any potential impact on this by the proposed development. Following the submission and review of these surveys the Surrey Wildlife Trust, informatives have been recommended in the event of planning permission being granted.

Neighbourhood Services: No objection has been raised and have confirmed that the placement of the bin collection would be acceptable.

Network Rail: No objection.

UK Power Networks: No objection.

SCC Minerals and Waste Authority: No objections raised.

## Representations:

Letters were sent to neighbouring properties on 24<sup>th</sup> June 2020. A total of 38 responses were received. Following the receipt of amended plans further notifications were sent to neighbouring properties on 30<sup>th</sup> October 2020. A further 4 responses were received. The following issues have been raised:

Overdevelopment	See paragraph 6.3-6.5
Out of character	See paragraph 6.3-6.5
Out of character with the surrounding area	See paragraph 6.3-6.5
Overdevelopment	See paragraph 6.3-6.5
Alternative location/ proposal	See paragraph 6.3-6.5
Overlooking and loss of privacy	See paragraph 6.10-6.14
Overbearing relationship	See paragraph 6.10-6.14
Overshadowing	See paragraph 6.10-6.14
No need for the development	See paragraph 6.3-6.5
Loss of/ harm to trees	See paragraph 6.15
Loss of private view	See paragraph 6.10-6.14
Hazard to highway safety	See paragraph 6.16-6.19

Inadequate parking	See paragraph 6.16-619
Increased traffic congestion	See paragraph 6.16-619
Inconvenience during construction	See paragraph 6.16-619
Drainage/ sewerage capacity	See paragraph 6.10-6.14
Property values	Not a material planning consideration
Harm to wildlife habitat	See paragraph 6.20.-6.24

## **1.0 Site and Character Appraisal**

- 1.1 The application site currently comprises a residential plot on the east side of Woodlands Road, to the south of Redhill. The site is occupied by a large detached dwellinghouse to the front of the plot, which is a locally listed building. The plot is large and rectangular in shape, with good tree cover along the boundaries and within the site, with the ground level sloping down from west to east towards the rear. The railway line runs past the rear of the site.
- 1.2 The area is predominantly residential in character, comprised of a mix detached, semi-detached and terraced dwellinghouses in a range of architectural styles and ages, with blocks of flats being found on the east side of the road. Two bus stops are located immediately outside the site and Earlswood Station is located approximately 220m to the south.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: Formal pre-application advice was not sought from the Local Planning Authority prior to the submission of the application.
- 2.2 Improvements secured during the course of the application: Amended plans have been submitted in response to concerns raised regarding elements of the design and scale of the building. Pre-liminary ecology surveys have also been sought.
- 2.3 Further improvements could be secured: Improvements to the scheme could be secured by way of suitably worded conditions and the imposition of informatives.

## **3.0 Relevant Planning and Enforcement History**

The following list of applications are relevant to the consideration of this application:

06/00343/F	Erection of a three storey building to the front of the site incorporating nine flats	Refused 5 <sup>th</sup> June 2006 Appeal Dismissed 18 <sup>th</sup> January 2007
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with three terraced  
properties to the rear  
and associated parking

#### 4.0 Proposal and Design Approach

- 4.1 This is a full application for the construction of a terrace of 3x4 bedroom dwellings to the rear of 29 Woodlands Road. The dwellings would be of a traditional architectural style, comprised of two pitched roof gables to the front and rear elevations. The overall height of the building would be 8.8m, a depth in to the rear of the site of 9.5m. The upper floor of the principal elevation is proposed to be tile hung, with stock and red facing brickwork for the lower floor and clay plain tile for the roof. Some modest detailing would feature to the gables, bargeboards, and ridge.
- 4.2 Externally each property would have an area of garden space to the rear, divided by fencing. A total of six parking spaces would be provided to the front to serve the proposed new dwellings, with an additional two spaces sited in front of the donor property to serve the existing dwelling. Additional landscaping would feature around the site and along the access road.
- 4.3 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.
- 4.4 Evidence of the applicant's design approach is set out below:

Assessment	Woodlands Road is identified as having a broad mix of housing types, ages and styles; however the majority are of the late Victorian/ Edwardian era. The road is considered to have a fairly consistent building line. No. 29 dates from late C19, probably 1880s. It is faced in clay plain tiles with decorative bands of club tiles to most of the front elevation and upper parts of the side and rear elevations. It has ornate bargeboards and finials on the front gables. It has sash windows. Part of the ground floor is faced in Reigate stone. It has a clay tiled roof and a decorative timber entrance porch. It is locally listed. Adjacent houses to the north date from slightly earlier. They are a pair of semidetached houses with gabled fronts in ornate brickwork with stone
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	dressings. The bricks are a mixture of stock and red facings.
	The presence of trees on and adjacent to the rear of the site.
Involvement	No community consultation is identified as having taken place.
Evaluation	It is not indicated that alternative development options have been considered.
Design	The design, form and scale of the proposed dwellings has been informed by the identified character of the surrounding area, which would be reflected by the proposed pitched roof, gabled form.

#### 4.5 Further details of the development are as follows:

Site area	0.1545 hectares
Site Density	19.4 d.p.h
Site density of surrounding area	Up to 60 dph
Existing Use	C3 Residential (single dwelling)
Proposed Use	C3 Residential (3x 4 bed dwellings)
Existing Parking Spaces	3
Proposed parking spaces	9
Parking standard	2 car parking spaces per 4 bed house in an area of high accessibility

## 5.0 Policy Context

### 5.1 Designation

Urban area

### 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS10 (Sustainable Development),

### 5.3 Reigate & Banstead Development Management Plan 2019

Design	DES1 DES2 DES4 DES5 DES6 DES8
Access and Parking	TAP1 Annex 4 Parking Standards
Climate change resilience and flooding	CCF1
Natural and Historic Environment	NHE3 NHE9

#### 5.4 Other Material Considerations

National Planning Policy Framework  
2019

National Planning Practice Guidance

Supplementary Planning Guidance

Surrey Design

Local Distinctiveness Design Guide

A Parking Strategy for Surrey

Parking Standards for Development

Other

Human Rights Act 1998

Community Infrastructure Levy

Regulations 2010

#### 6.0 **Assessment**

6.1 The application seeks permission for the erection of a terrace of 3x4 bedroom dwellings.

- Design Assessment;
- Amenity of future occupiers;
- Impact on neighbouring amenity;
- Trees and Landscaping;
- Highway Matters;
- Ecology;
- CIL
- Affordable Housing

##### Principle of Development

6.2 The site is located within the urban area where there is not an in principle objection to the introduction of new residential development. In considering the appeal against the refusal of application 06/00343/F, which included a terrace of three dwellings at the rear, the inspector considered that the site is suitable for redevelopment in principle 'but only if the design is good enough.' The Inspector found the rear dwellings in that proposal to be cramped, by virtue of their proximity to the rear of the site and railway embankment at 6-7 metres distant from the rear windows. Therefore the acceptability of the proposal rests with consideration of the design of the proposal and its impact on the character of the area, impact on the amenity of neighbouring properties, parking and highway implications, as well as any impact on trees, existing landscaping and ecology.

##### Design Assessment

6.3 As described earlier in this report, the design of the dwellings has been informed by the character of both the Victorian/ Edwardian style houses in the

immediate vicinity of the site, particularly the locally listed frontage building, whilst acknowledging the mix of architectural styles along Woodlands Road and its surroundings. The building would be representative of the typical Surrey vernacular form, with its pitched roof gables with architectural detailing to the bargeboards, tile hung first floor and Tawney colour palette of materials would be appropriate within the context of the area and not harmful to its character. The siting of the building to the rear of the plot at a lower ground level than the donor property would ensure that views of the development from the wider road are minimal. Even if it were more visible its appearance would not be harmful given its design. It should be noted that in consideration of the 2006 dismissed scheme, the inspector found no issue with the architectural design of the building, which was similar to the proposed and greater in height by 1m, with a taller, more dominant roof form and greater degree of bulk. The main concern of the inspector in this case was the angular siting of the building, which resulted in a particularly cramped form of development, with plot 3 being particularly small. It is considered that this has been overcome by the current proposal. The Conservation Officer is satisfied that the development would not impact on the character and setting of the listed building to the front of the site. Conditions controlling the materials and detailing be required to be discharged in the event of permission being granted. Further conditions removing permitted development rights for extensions, dormers, rooflights and solar panels would be included.

- 6.4 The footprint of the proposed building would be roughly parallel with the donor property and Woodlands Road to the west. The depth of the building would be 9.5m (approx.. 0.10m deeper than the appeal scheme). It would be sited 2.5m from the north boundary at the closest point and 1.8m from the south boundary. This level of spacing around the building is considered to be acceptable, conforming with the spacing between buildings observed within Woodlands Road. Whilst the provision of parking would introduce hardstanding in to the middle of the site, it would be a significant reduction over the previous scheme, and it is considered that there would be sufficient space around the development for landscaping to assist with softening the appearance of the harder landscaped elements. The distance to the rear boundary would be 9.5m for plot 1, increasing to 10.5m for plot 3, following the line of the rear boundary, affording an area of rear garden space that would increase the level of spaciousness around the building. This represents a material improvement upon the 2006 scheme, which at 6-7 metres distant from the railway embankment was adjudged to be cramped. The proposed 9.5-10 metre garden depths are considered to be more acceptable in terms of providing sufficient depth of site, outlook and amenity space for the residents of the proposed development.
- 6.5 Its is considered that the design of the development and its impact on the character of the area would be acceptable and in accordance with Policy DES1 of the Development Management Plan 2019.

Amenity for future occupants



- 6.6 It is a fundamental objective of planning policy and stated within the National Planning Policy Framework that we provide high quality housing that is well designed and built to a high standard. The advice is amplified further by policies DES2 and DES5 of the Development Management Plan, which requires developments to demonstrate that dwellings have been designed to ensure that a good standard of amenity for all existing and future occupants and meet the minimum relevant nationally described space standards and be arranged to ensure that habitable rooms are arranged to have an acceptable outlook and where possible receive direct sunlight.
- 6.7 The proposal is for 3x4 bed dwellings. Plots 1 and 3 would be 3 storey, whilst plot 2 would be two storey with accommodation in the roof. Nationally Described Space Standards require a 4 bedroom two storey dwelling to provide a minimum of 103 sq. metres. Plots 1 and 3 would have a floor area of 146 sq. m, whilst plot 2 would have a floor area of 147sq.m. The proposal would therefore comply with the requirements of Policy DES5 in this regard.
- 6.8 Externally each property would have a garden. These would be varied in size, plot 1 being 104 sq. m, plot 2 64 sq. m and plot 3 148 sq. m, with a depth of garden ranging from 10-16m, following the angled rear boundary. These would be smaller than the existing dwelling occupying the site, however they would not be wholly out of character with the surrounding area in this regard. Numbers 31 and 33 Woodlands Road, also large semi-detached dwellings, are noted to have particularly small rear gardens. Properties to the north of Woodlands Road are identified as having gardens of a similar depth to those being proposed.
- 6.9 It is considered that the living environment for future occupants would be acceptable. Each dwelling would also have access to appropriate levels of communal amenity space and would accord with provide a level of amenity space that would accord with National Space Standards. In this regards the proposal would accord with DMP Policy DES5.

Impact on neighbouring amenity

- 6.10 The proposed building would be sited to the north of numbers 2-12 St Johns Road, a row of semi-detached properties with long rear gardens (between 16-20m in depth to the shared boundary with the site). These properties are sited at a lower ground level than the proposed dwellings. The boundary between the two sites is well screened by existing trees and other vegetation, which would reduce the level of view of the building from the neighbouring properties. It is noted that two of these trees (a Sycamore and an Ash) would be removed however they would be replaced as part of any future landscaping scheme. The proposed dwellings would be 2m away from the shared boundary at the closest point, widening to 3.5m towards the rear, and in excess of 20m from the rear elevations of the neighbours. Given this distance, coupled with the amount of screening, it is considered that the development would not be significantly overbearing in nature. There are no upper floor side windows proposed therefore there is no concern with regard to overlook and loss of privacy to these neighbours.

- 6.11 With regard to the relationship with the donor property, 29 Woodlands Road, this would sit above the application site due to the change in ground levels from front to rear. There would be a distance of 21m between the two properties. Number 29 features rear windows which would look towards the development and likely serve habitable rooms. A series of bedroom windows would face towards the neighbour's rear garden area. Whilst some overlooking would undoubtedly occur, this would not be an unusual relationship and has been deemed acceptable for many backland development schemes in the Borough such as this. Given the separation distance and lower level it is not considered that there would be significant harm to the amenity of the donor property.
- 6.12 Number 27 to the north of the donor property is a large detached building which appears to be divided into flats. Again this property would be sited at a higher ground level and the boundary would be well screened by vegetation, the existing mature hedge being retained. Whilst some views of the development would be afforded from the rear windows of number 27, the separation distance, lower ground level and the extensive boundary screening would ensure that this is minimal.
- 6.13 Number 31 Woodlands Road would be to the south of the proposed access. The distance between the two elements would be approximately 9.5m although it is noted that the neighbouring garage is positioned on the shared boundary. Whilst the additional traffic movements may result in some increased noise disturbance the sites would be divided by a fence. It should be noted that appeal decisions have considered such relationships with new accessways acceptable (19/00706/F/AP) and given the small number of proposed dwellings there would not be a significant amount of traffic movement throughout the day that would be detrimentally harmful to the neighbours' enjoyment of their property.
- 6.14 Objection has been made on the grounds that the development would result in a poor outlook and loss of view for neighbouring properties. It is recognised that the development would result in change, but it is not considered that this would be materially harmful as the proposed dwellings would be a sufficient distance away from neighbouring properties to not impact on outlook. It is also the case that a 'right to a view' is not a material planning consideration in the assessment of a planning application. Regarding inconvenience and increased noise and disturbance during construction, whilst it is accepted that this can be an issue for residents, it is a temporary impact, rather than being capable of a reason for refusal and would be mitigated by condition. As regards to concern about impact on property values, this is not a material planning consideration that can be taken into account. With regard to the development being in conflict with a covenant, this is not a material planning consideration that can be taken into account. Issues regarding drainage would be considered under Building Regulations Legislation. The Site is within Flood Zone 1 where this development is acceptable in principle but a condition is suggested with regards providing appropriate surface water attenuation.

#### Trees and Landscaping

- 6.15 There are no protected trees within the site however there are a large number of trees within the site and along the boundaries. The Councils Tree Officer has assessed the application and the submitted arboricultural information, which demonstrates that none of the trees to be removed are significant specimens and their removal will not have an impact on the appearance of the local landscape. Boundary trees are shown to be retained which will provide an established screen between the neighbours and future occupants whilst to the rear trees would not be under pressure from the development given the increased separation distances. In order to enhance the appearance of the site the submission of a landscaping scheme prior to commencement of development would be required should planning permission be granted. The submitted site layout suggests the intention to provide additional tree planting to the south of the site, which would provide screening of the side from the properties on St Johns Road.

#### Highway Matters

- 6.16 The site is located within an area of high accessibility as defined within Annex 4 of the Development Management Plan. The proposal is for three 4 bedroom properties. On this basis 6 parking spaces would be required to be provided for future residents. The development proposed 6 spaces in front of the dwelling and would therefore comply in this regard. Two further spaces would be provided adjacent to the access for the donor property.
- 6.17 The Highway Authority has commented on the proposal and have recommended conditions requiring the construction of the proposed access road in accordance with the approved plans, the provision of pedestrian inter-visibility splays, the provision of 9 parking spaces in accordance with the approved plans, the submission of a construction transport management plan prior to commencement of development, and the installation of electric vehicle charging points.
- 6.18 It is noted that objection has been made with regard to the impact of the proposed development on the current on-street parking taking place on Woodlands Road, as well as with regard to potential highway safety implications posed by the proximity of the application site to 2 bus stops, one on each side of the road. The County Highway Authority has deemed this arrangement to be acceptable and would not result in a highway safety issue. If there is a bus waiting at either bus stop then drivers from the site heading in that direction will have to wait behind the bus until it moves, as happens in many locations in the borough. If there is no bus then drivers can move freely. The situation regarding the bus stop is an existing situation. The access will have adequate sightlines and geometry to support the proposed development.
- 6.19 In order to manage the construction process, a condition requiring the submission and approval of a Construction Transport Management Plan would be included in any grant of permission.

#### Ecology

6.20 This site is not subject to any formal designations with regard to ecology; however the site is well landscaped and has the potential to provide suitable habitats for a range of species. Policy NHE2 of the Development Management Plan advises that throughout the borough, development proposals will be expected to:

a. retain and enhance other valued priority habitats and features of biodiversity importance; and

b. be designed, wherever possible, to achieve a net gain in biodiversity. Where a development will impact on a priority habitat or species, or protected species, and mitigation cannot be provided on site in an effective manner, developers may be required to offset the loss by contributing to appropriate biodiversity projects elsewhere, in a location agreed with the Council.

Preliminary ecology surveys have been submitted in support of the application. It is noted that a section of single storey flat roofed extension associated with the main house at 29 Woodlands Road will be removed to facilitate access to the site. The extension does not appear to have been assessed for its potential to host roosting bats. From the information provided the extension does not appear to have features typically suitable for bats. However, in the absence of ecological survey information it has been recommended that a precautionary approach to works is implemented to avoid contravention of protected species legislation. Should permission be granted, the applicant would be required to implement a precautionary approach to works. Works affecting the extension should be undertaken carefully and by hand. If a bat or evidence of bats is found on site, prior to or during works, work should cease immediately and advice sought from Natural England or a qualified specialist.

6.21 Works carried out to any trees within the site that have the potential to provide roosting habitats for bats would be expected to be carried out in accordance with the precautionary working methods as outlined within the submitted ecology survey.

6.22 Sensitive lighting will also be kept to a minimum to ensure against the disturbance of nocturnal animals. Additional precautionary measures as outlined within the ecology survey with regard to the presence of badgers, breeding birds and reptiles that may be identified during the course of development will be expected to be adhered to.

6.23 The site has the potential to provide habitats for hedgehogs. As well as proceeding in a precautionary manner so as not to disturb existing habitats, in the event of permission being granted, wildlife permeable fencing should be incorporated into the scheme. This can be secured as part of a pre-commencement landscaping condition.

6.24 Subject to development being carried out in accordance with the advice contained within the Ecology Survey and informatives recommended by Surrey

Wildlife Trust, the proposal is considered to comply with Policy NHE2 of the Development Management Plan 2019.

### CIL

- 6.25 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, roads, public transport and community facilities which are needed to support new development. This development would be CIL liable although, the exact amount would be determined and collected after the grant of planning permission.

### Affordable Housing

- 6.26 Core Strategy Policy CS15 and the Council's Affordable Housing SPD require financial contributions towards affordable housing to be provided on housing developments of 1-9 units. However, the 2018 NPPF makes clear such contributions should not be sought from developments of 10 units or less.
- 6.27 In view of this the Council is not presently requiring financial contributions from applications such as this resulting in a net gain of 10 units or less. The absence of an agreed undertaking does not therefore warrant a reason for refusal in this case.

## **CONDITIONS**

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Proposed Plans	2007/PL 05	A	27.10.2020
Floor Plan	2007/PL 04	A	27.10.2020
Elevation Plan	2007/PL 06	A	27.10.2020
Elevation Plan	2007/PL 07	A	27.10.2020
Proposed Plans	2007/PL 08	A	27.10.2020
Elevation Plan	2007/PL 06	A	27.10.2020
Section Plan	2007 / PL 12		20.07.2020
Proposed Plans	2007 / PL 11		20.07.2020
Location Plan	2007/PL 01		15.06.2020
Site Layout Plan	2007/PL 03		15.06.2020
Survey Plan	2007/PL 09		15.06.2020
Site Layout Plan	2007/PL 10		15.06.2020
Block Plan	2007/PL 02		15.06.2020

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. Notwithstanding the drawings, the proposed external finishing materials and details shall be carried out using the external facing materials and details specified below and there shall be no variation without the prior approval in writing of the Local Planning Authority;

a) The windows are white vertically sliding sash set back behind the reveal at one brick depth with a glazing bar of traditional profile and painted masonry cills.

b) The roof shall be of dark handmade or handcrafted sandfaced clay plain tile with decorative clay ridge tiles to match existing. The tile hanging shall be of handmade sandfaced plain clay tiles with two rows of fish scale tiles to every row of plain, of light colour to match the Victorian tile hanging and shall be tile hang above first floor to the front and the whole of the sides.

c) All brickwork shall be of dark red handmade sandfaced clay, in Flemish bond to the front and sides.

d) The external doors shall be of painted timber.

e) All fencing shall be vertical close boarded fencing with timber gravel boards and posts.

f) Before any works above ground level details of a bin store at the front entrance shall be submitted to and approved in writing by the LPA.

g) All rainwater goods shall be of black painted cast metal or black plastic cast iron profile with the gutter fascia of no greater height than the gutter, with white painted timber open rafter feet.

h) Before the new houses are occupied the shortened single side extension to the existing house shall be made good to match existing.

i) The new hedge shall be of privet.

j) All bargeboards shall be of white painted timber of profile and detail to match the front bargeboards on the existing house.

k) The drive and parking areas shall be of fixed flint pea shingle.

l) All photovoltaics and solar panels shall either be within the crown of the new houses and no higher than the ridge tiles or on the rear plane of the roofs of the new houses with black frames in a symmetrical and rectilinear form.

m) All charging points shall be black finished.

n) Before any works above ground level a specification for the repair of the existing house shall be submitted to and approved in writing by the LPA and the repairs shall be carried out before the occupation of the new houses.

Reason: To preserve the character and setting of the Locally Listed Building in accordance with Policies DES1 and NHE9 of the Development Management Plan 2019.

4. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015, (or any Order revoking and re-enacting that Order with or without modification), no first floor windows, dormer windows or rooflights other than those expressly authorised by this permission shall be constructed.

Reason: To ensure that the development does not affect the amenity of the neighbouring properties by overlooking and to protect the visual amenities of the area in accordance with Policy DES1 of Development Management Plan 2019.

5. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no extensions, photovoltaics, solar panels, rooflights or dormers permitted by the 2015 Order or its successors, shall be constructed (other than those expressly authorised by this permission) without an application and approval by the local planning authority.

Reason: To ensure that the development does not affect the amenity of the neighbouring properties, and prevent harm to the character and setting of the Locally Listed Building, in accordance with Policies DES1 and NHE9 of the Development Management Plan 2019.

6. No development shall commence until details of hard and soft landscaping is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include frontage tree and hedge planting and any other existing or proposed, soft or hard, landscaping in the front garden area, or adjacent to boundaries where appropriate. The soft landscape details shall include an establishment maintenance schedule for a minimum of 2 years, full planting specifications, planting sizes & densities. Upon implementation of the approved development all the landscaping works shall be carried out in strict accordance with the landscape details as approved, and these shall be completed, before building completion, occupation or use of the approved development whichever is the earliest.

If any of the new or existing tree/s or hedge/s, detailed and approved under this condition, are removed, die, or become significantly damaged or diseased within 5 years of completion, it/they shall be replaced before the expiry of one calendar year, to a planting specification agreed in writing by the Local Planning Authority. The hedges detailed shall be retained at a minimum height of 1 metre, or if new, once grown to this height thereafter.

Reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with

policies NHE3, DES1 and DES3 of the Reigate and Banstead Development Management Plan 2019.

7. No development shall commence including demolition and or groundworks preparation until a detailed, scaled finalised Tree Protection Plan (TPP) and the related finalized Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings and location of site offices. The AMS shall also include a pre commencement meeting, supervisory regime for their implementation & monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3, DES1 and DES3 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

8. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. These details should include measures to encourage biodiversity and wildlife, and allow wildlife to move into and out of gardens, and include Hedgehog friendly gravel boards where appropriate. Details of the 'wildlife friendly' measures should be provided with the submission of the details for approval. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: To preserve and enhance the visual amenity of the area, protect neighbouring residential amenities, and provide access to habitats for wildlife, in accordance with Reigate and Banstead Development Management Plan Policies DES1 and NHE3.

9. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
  - a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
  - b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations



The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

10. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:

- a) A broadband connection accessed directly from the nearest exchange or cabinet
- b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

11. No part of the development shall be occupied unless and until the existing vehicular access to Woodlands Road has been revised in accordance with the approved plan numbered 2007 PL 02.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

12. Prior to The development hereby approved shall not be commenced unless and until a pedestrian inter-visibility splay measuring 2m by 2m has been provided on each side of the access to Woodlands Road the depth measured from the back of the footway (or verge) and the widths outwards from the edges of the access in accordance with a scheme to be submitted to and approved in writing with the Local Planning Authority. No obstruction to visibility between 0.6 and 2.0 metres in height above ground level shall be erected within the area of such splays.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

13. The development hereby approved shall not be commenced unless and until a pedestrian inter-visibility splay measuring 2m by 2m has been provided on each side of the access to Woodlands Road the depth measured from the back of the footway (or verge) and the widths outwards from the edges of the access in accordance with a scheme to be submitted to and approved in writing with the Local Planning Authority. No obstruction to visibility between 0.6 and 2.0 metres in height above ground level shall be erected within the area of such splays.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

14. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plan numbered 2007 PL02 for a minimum of 9 cars to be parked. Thereafter the parking area shall be retained and maintained for its designated purpose.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

15. No development shall commence until a Construction Transport Management Plan, to include details of:

- (a) parking for vehicles of site personnel, operatives and visitors
- (b) loading and unloading of plant and materials
- (c) storage of plant and materials
- (d) programme of works (including measures for traffic management)
- (e) provision of boundary any hoarding behind visibility zones
- (g) vehicle routing
- (h) measures to prevent the deposit of materials on the highway
- (i) construction hours

has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

16. The development hereby approved shall not be occupied unless and until each of the proposed dwelling(s) are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) in accordance with a scheme to

be submitted and approved in writing by the Local Planning Authority and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

17. Notwithstanding the submitted plan numbered 2007 PL 02 no part of the development shall be occupied unless and until the western end of the refuse collection point has been relocated to a point 25 metres from the highway in accordance with a revised scheme to be submitted to and approved in writing with the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

18. The development shall only be completed in accordance with the measures as set out in the ecological appraisal accompanying the application.

Reason: To protect the important species on the site in accordance with Policy NHE2 of the Reigate and Banstead Borough Development Management Plan 2019, Natural England standing advice and the provisions of the NPPF.

19. The development hereby permitted shall not commence until details of the design of a surface water drainage scheme has been submitted to and approved in writing by the local planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDs, NPPF and Ministerial Statement on SuDs. The required drainage details shall include:

- a) Evidence that there is no risk of contamination through the infiltration SuDs
- b) Evidence that the proposed final solution will effectively manage the 1 in 30 and 1 in 100 (+40% allowance for climate change) storm events and 10% allowance for urban creep, during all stages of the development (Pre, Post and during), associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 37.8l/s.
- c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels and long and cross sections of each element including details of any flow restrictions and maintenance / risk reducing features (silt traps, inspections chambers etc)

- d) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational
- e) Details of drainage management responsibilities and maintenance regimes for the drainage system
- f) A plan showing exceedance flows (ie during rainfall greater than design events or during blockage) and how property on and offsite will be protected. This should include details of how surface water run-off entering the site from the bunded northern boundary will be intercepted.

The development shall be completed in accordance with the approved details and thereafter maintained.

Reason: To ensure the design meets the technical standards for SuDs and the final drainage design does not increase flood risk on or off site in accordance with, Policy CS10 of the Core Strategy 2014, Policies DES9 and CCF2 of the Development Management Plan 2019 and the 2019 NPPF.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, a 140 litre wheeled bin conforming to British Standard BSEN840 and a 60 litre recycling box should be provided for the exclusive use of the occupants of that dwelling. Prior to the initial occupation of any communal dwellings or flats, wheeled refuse bins conforming to British Standard BSEN840, separate recycling bins for paper/card and mixed cans, and storage facilities for the bins should be installed by the developer prior to the initial occupation of any dwelling hereby permitted. Further details on the required number and specification of wheeled bins and recycling boxes is available from the Council's Neighbourhood Services on 01737 276501 or 01737 276097, or on the Council's website at [www.reigate-banstead.gov.uk](http://www.reigate-banstead.gov.uk). Bins and boxes meeting the specification may be purchased from any appropriate source, including the Council's Neighbourhood Services Unit on 01737 276775.
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;

- (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
- (c) Deliveries should only be received within the hours detailed in (a) above;
- (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
- (e) There should be no burning on site;
- (f) Only minimal security lighting should be used outside the hours stated above; and
- (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

5. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. Replacement planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate substantial sized trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Semi- Extra Heavy Standard size with initial planting heights of not less than 4m with girth measurements at 1m above ground level in excess of 14/16cm.
6. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
7. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
8. The permission hereby granted shall not be construed as authority to carry out any works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will

need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see [www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice](http://www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice).

9. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
10. The western end of the refuse collection point should be located 25 metres from the highway on the grass area west of plot 3 as shown on the plan numbered 2007 PL 02 to allow cars to park in front of the existing dwelling.
11. The applicant is advised that development should take place in a precautionary manner so as to avoid harm to bats. It is advised that as a precautionary measure the gaps identified on the sheds and Ivy should be removed carefully by hand.
12. If a bat or evidence of bats is found on site, prior to or during works, work should cease immediately, and advice sought from Natural England or a qualified specialist. Further surveys and mitigation may be required.
13. Works to any trees identified as offering low bat roost potential should only progress in accordance with the recommendations made in section 5.2.4 of the above referenced Preliminary ecology appraisal Report.
14. The development should only progress in accordance with the recommendations for badgers made in Section 5.1.1 of the above referenced Preliminary ecology appraisal Report.
15. The development should only progress in accordance with the recommendations for birds made in Section 5.2.3 of the above referenced Preliminary ecology appraisal Report.
16. With regard to amphibians and reptiles identified on site, works should progress in a precautionary manner in accordance with recommendations made in Sections 5.2.2 and 5.2.6 of the above referenced Preliminary ecology appraisal Report. We advise that the clearance of any logs, brash, stoned rocks or piles of similar debris should be undertaken carefully and by hand.
17. The pond should be removed in a precautionary manner and by hand to prevent harm to any protected species that may be present. If any protected

species are identified then works should cease immediately and advice sought from a suitably qualified ecologist.

18. The development should only progress in accordance with the recommendations for hedgehogs made in Section 5.2.2 and 5.2.6 of the above referenced Preliminary ecology appraisal Report.
19. Ecological enhancements should be incorporated into the proposals to help provide an overall net gain for biodiversity.
20. The applicant should ensure that the proposed development will result in no net increase in external artificial lighting in line with recommendations of the Bat Conservation Trusts document entitled 'Bats and Lighting in the UK – Bats and The Built Environment Series'.

## **REASON FOR PERMISSION**

The development hereby permitted has been assessed against the NPPF 2019 and Development Management Plan policies DES1 DES2 DES5 DES8 DES9 CCF2 TAP1 NHE2 NHE3 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

# 20/01212/F - 29 Woodlands Road, Redhill







Do not scale from this drawing, except for planning purposes.

N

0 1 2 4 6 8 10m

Existing trees

New trees

Rev	Date	Description

**KAA**  
KA Architectural Ltd  
kaa1@email.com

Date  
May 2020

Scale  
1:200 @ A1

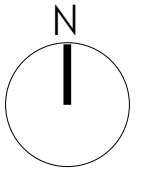
Client  
Beaufort Homes Ltd

Project  
29 Woodlands Road  
Redhill  
RH1 6EY

Title  
Proposed Site Plan

Drawing No.  
2007 / PL 03

Do not scale from this drawing, except for planning purposes.



Rev	Date	Description
<b>KAA</b> KA Architectural Ltd kaa1@email.com		
Date	May 2020	
Scale	1:500 @ A3	
Client	Beaufort Homes	
Project	29 Woodlands Road Redhill RH1 6EY	
Title	Block Plan	
Drawing No	2007 / PL 02	



West Elevation



South Elevation



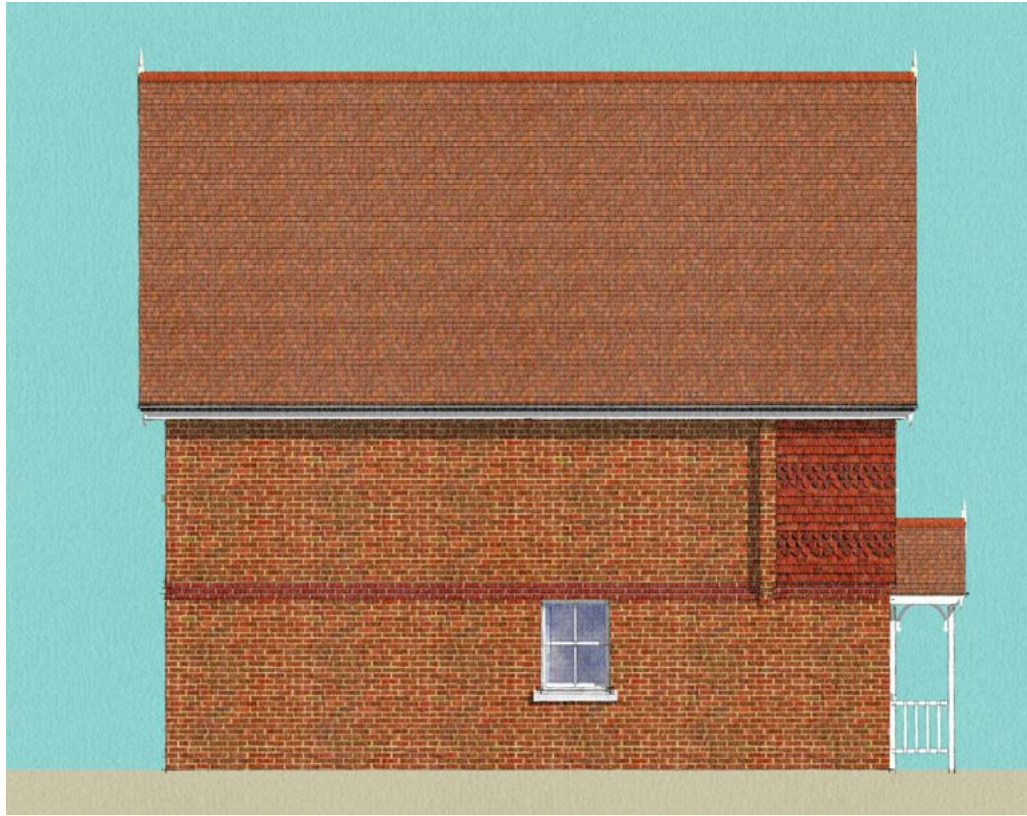
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Rev	Date	Description
<b>KAA</b>		
KA Architectural Ltd kaa1@email.com		
Date		
May 2020		
Scale		
1:100 @ A3		
Client		
Beaufort Homes		
Project		
29 Woodlands Road Redhill RH1 6EY		
Title		
Elevations: West & South		
Drawing No		
2007 / PL 06 A		



East Elevation



North Elevation



A	27.10.20	Gable width amended
Rev	Date	Description
<b>KAA</b>		
KA Architectural Ltd kaa1@email.com		
Date		
May 2020		
Scale		
1:100 @ A3		
Client		
Beaufort Homes		
Project		
29 Woodlands Road Redhill RH1 6EY		
Title		
Elevations: East & North		
Drawing No		
2007 / PL 07 A		



West Elevation

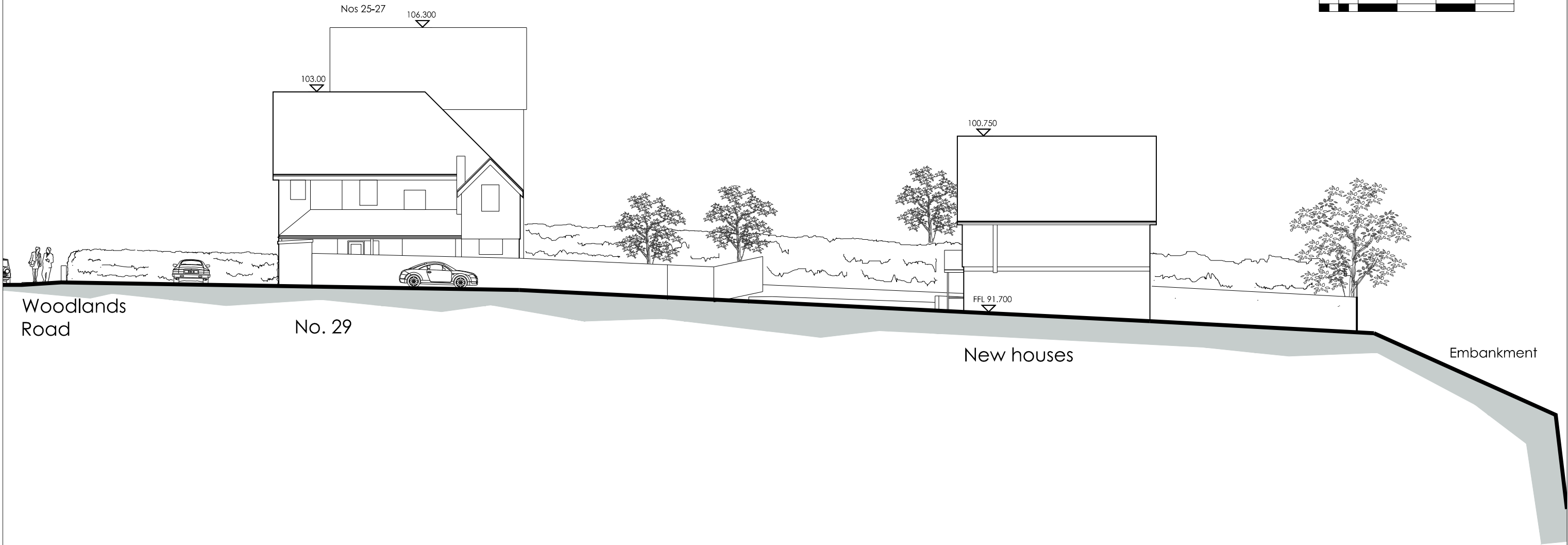
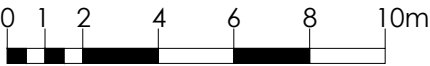


South Elevation



A	27.10.20	Gable width amended
Rev	Date	Description
<b>KAA</b>		
KA Architectural Ltd kaa1@email.com		
Date		
May 2020		
Scale		
1:100 @ A3		
Client		
Beaufort Homes		
Project		
29 Woodlands Road Redhill RH1 6EY		
Title		
Elevations: West & South		
Drawing No		
2007 / PL 06 A		

Do not scale from this drawing, except for planning purposes.



Rev	Date	Description

**KAA**

KA Architectural Ltd  
kaa1@email.com

Date	Jul 2020
Scale	1:200 @ A3
Client	Beaufort Homes
Project	29 Woodlands Road Redhill RH1 6EY
Title	Site Section

Drawing No	2007 / PL 12
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A	27.10.20	Gable width amended
Rev	Date	Description
<b>KAA</b>		
KA Architectural Ltd kaa1@email.com		
Date		
May 2020		
Scale		
NTS		
Client		
	Beaufort Homes	
Project		
	29 Woodlands Road Redhill RH1 6EY	
Title		
	Perspective View	
Drawing No		
	2007 / PL 08 A	





Rev	Date	Description
<div><div><div>KAA</div><div>KA Architectural Ltd kaa1@email.com</div></div><div><div>Date</div><div>JUL 2020</div></div><div><div>Scale</div><div>NTS</div></div><div><div>Client</div><div>Beaufort Homes</div></div><div><div>Project</div><div>29 Woodlands Road Redhill RH1 6EY</div></div><div><div>Title</div><div>CGI</div></div><div><div>Drawing No</div><div>2007 / PL 11</div></div></div>		