

 <p>Reigate &amp; Banstead BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate</p>	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	24 April 2024
	<b>REPORT OF:</b>	HEAD OF PLANNING
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<b>AGENDA ITEM:</b>	6	<b>WARD:</b> Nork

<b>APPLICATION NUMBER:</b>	23/02289/F	<b>VALID:</b>	22/11/2023
<b>APPLICANT:</b>	Trinity Square Developments	<b>AGENT:</b>	
<b>LOCATION:</b>	<b>LAND TO THE REAR OF 59 AND 61 NORK WAY, BANSTEAD</b>		
<b>DESCRIPTION:</b>	<b>Erection of 2 x 4-bedroom detached dwellings with associated access, parking and landscaping.</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

**This application is referred to Committee in accordance with the Constitution as the application site is for net 2 dwellings**

### **SUMMARY**

The application is for the erection of two x 4-bedroom detached dwellings to the rear of nos 59 and 61 Nork Way following the sub-division of the plots along with off road car parking spaces for 2 cars for each property to the front of the houses and rear garden laid to lawn to the rear. A new access road, adjoining the recently constructed drive to the front of no.61 Nork Way is proposed to be constructed along the boundary with no.63 Nork Way (doctor's surgery.)

The resultant plot sizes and widths would be commensurate with the surrounding area and the properties would be in keeping with the 1920/30s vernacular seen in the surrounding area.

In terms of neighbour amenity, the proposed dwellings would be sufficiently spaced away from the donor properties and other properties in Nork Way and Rowan Walk so as not to materially impact the amenity of those properties. There would be no significant overlooking due to the depth of the proposed gardens.

The access road to Nork Way has already been partially constructed as a private driveway and would continue to the side of no 61 Nork Way. There is an opportunity for the access road to be further landscaped and this can be controlled by condition.

There would be sufficient on-site parking to meet current standards and conditions have been proposed to replace the existing on street parking to ensure that there would be no exacerbation of parking on or near the site.

**RECOMMENDATION(S)**

Planning permission is GRANTED subject to conditions.

## Consultations:

Highway Authority: No objections subject to conditions relating to the access, parking, and the provision of electrical charging points for cars and storage and charging points for e-bikes along with facilities for refuse collection.

Tree Officer: As the access drive is already in place the key arboricultural feature T3 there will be no further work required that may affect it structurally. The remaining trees are low quality and will need to be removed to facilitate this development as they are located in the rear garden, their removal will not have a noticeable impact on the appearance of the immediate area. Regardless of their stature replacement planting is required that will enhance the site and the wider landscape, for this site there is limited scope for this to happen. Despite these concerns from an arboricultural view there is no reason to object to this application.

Surrey Wildlife Trust: recommends that the ecology report is followed during the course of construction.

Nork Residents' Association: "As Chairman I object to this application on behalf of Nork Residents' Association. This is grossly overdevelopment. It will have an adverse impact on the donor properties by overlooking and, in the case of number 61, disturbance from vehicles using the new access driveway. Its proximity will have an adverse impact on Nork Clinic next door from vehicles using proposed access driveway. With a driveway immediately adjacent to the exit from the Clinic it will present danger to vehicles leaving the Clinic. It will also reduce parking which is used daily by patients visiting the clinic. It will present a danger to pedestrians walking into Nork Clinic. The parked vehicles outside the property leading up to the shops make Nork Way a single track road at this point. There are also vehicles leaving Warren Road which add to the dangerous road conditions. There are frequent "near misses" and vehicles using the proposed driveway would often present a hazard to road users in each direction. The proposed development would be overlooking existing properties in Willow Close. This will have an adverse impact on the enjoyment by residents of Willow Close of their gardens through both overlooking and disturbance from vehicles accessing the proposed new properties. Finally, there will be a significant loss of biodiversity."

## Representations:

Letters were sent to neighbouring properties on 27 November 2023. Seventy-seven letters of representation from local residents have been received raising the following concerns:

Issue	Number	Response
Overlooking and loss of privacy	39	See paragraphs 6.12 – 6.17
Overshadowing	16	See paragraphs 6.12- 6.17
Noise & disturbance	38	See paragraph 6.15

Overbearing relationship	30	See paragraphs 6.12 – 6.17
Overdevelopment	64	See paragraphs 6.4 – 6.7
Out of character with surrounding area	39	See paragraphs 6.4 – 6.8
Poor design	13	See paragraph 6.7
Loss of/harm to trees	48	See paragraphs 6.21 – 6.22
Harm to Green Belt/countryside	20	Site is not within the Green Belt
Harm to wildlife habitat	47	See paragraph 6.23 – 6.24
Harm to Conservation Area	15	Site is not within a conservation area
Harm to listed building	1	There are no listed buildings in the vicinity
Drainage/sewage capacity	29	See paragraph 6.26
Flooding	19	See paragraph 6.26
Property devalue (a non planning matter)	2	This is not a material planning consideration
No need for the development	56	Application is assessed on its own merits
Loss of private view	29	This is not a material planning consideration
Alternative location/ proposal preferred	6	Only the application in front of the Council can be assessed.
Increase in traffic and congestion	69	See paragraphs 6.18 – 6.20
Inadequate parking	57	See paragraphs 6.18 – 6.20
Hazard to highway safety	58	See paragraphs 6.18 – 6.20

## **1.0 Site and Character Appraisal**

1.1 The application site currently consists of two detached dwellings built in the 1920s/30s that are set within extensive and deep plots. The proposal is to split the rear half of the plots to allow for two new dwellings with access to the side of 61 Nork Way. The rear gardens were previously well landscaped with a number of trees and hedging. There is a mixture of boundary treatments, including close boarded fencing and hedging. The site is relatively flat although it is noted that the properties to the south-western boundary are at a lower level.

1.2 The surrounding area is largely residential, although it is noted that the adjoining property to the south, at 63 Nork Way, is a doctors' surgery. The predominant house types are typically of the 1930s and of two storeys although to the rear of the site is a recent back garden development and to the north-east of the site are bungalows.

1.3 At the time of the site visit, a new driveway following the proposed access road has been built up to the side of no.61 Nork Way.

## **2.0 Added Value**

2.1 Improvements secured at the pre-application stage: Further ecology information

2.2 Further improvements could be secured: Materials, landscaping, tree protection condition, broadband condition, water condition, highways conditions, bin store information, ecology conditions

## **3.0 Relevant Planning and Enforcement History**

3.1 59 Nork Way  
96/14080/CU - Change of use of dental surgery to living accommodation – approved with conditions  
97/13220/F - Proposed single storey rear conservatory – approved with conditions.  
22/02660/CLP - Removal of existing conservatory / replacement of windows / addition of velux window in loft as part of proposed loft conversion – permitted development

3.2 61 Nork Way  
22/02661/CLP - Removal of existing extensions / replacement of windows / addition of dormer window in loft as part of proposed loft conversion – permitted development.

## **4.0 Proposal and Design Approach**

4.1 The proposal is for the erection of two 4 bedrooms detached dwellings to the rear of nos 59 and 61 Nork Way along with associated access, parking, and landscaping.

4.2 The design of the properties broadly reflects the properties in the vicinity that date primarily from the 1930s with a traditional design and materials with tiled roofs, brickwork and render with hipped gable features to the front.

4.3 Parking has been provided for each property with two off road car parking spaces per property.

4.4 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:

- Assessment;
- Involvement;
- Evaluation; and
- Design.

4.5 Evidence of the applicant's design approach is set out below:

Assessment	The application site is situated within the urban area of Banstead, where we have demonstrated there is a presumption in favour of sustainable development. There is also a principle from recent planning decisions, that new residential development is acceptable in land use terms in rear gardens
Involvement	No community consultation took place.
Evaluation	N/A
Design	Inspiration has been sought from the shapes and forms of the adjacent developments. The proposal sets out to create dwellings which are in keeping with the surrounding art and craft buildings which are subservient to the existing dwellings on the plot.

4.6 Further details of the development are as follows:

Site area	0.25 hectares
Proposed Site density	16 dwellings per hectare
Existing Site density	Established dwellings 9 dph Willow Close 40 dph Nork Gardens 35 dph
Proposed parking spaces	4
Parking standard	4
Net increase in dwellings	2

## 5.0 Policy Context

### 5.1 Designation

Urban Area

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS4 (Valued Townscapes and Historic Environment)  
CS10 (Sustainable Development),  
CS11 (Sustainable Construction),  
CS15 (Affordable Housing)

5.2 Reigate & Banstead Development Management Plan 2019

Natural Environment	NHE2, NHE3
Design, Character, and amenity	DES1, DES2, DES5, DES8
Transport, Access, and parking	TAP1
Climate Change resilience	CCF1
Infrastructure to support growth	INF3

5.3 Other Material Considerations

National Planning Policy Framework	
Supplementary Planning Guidance	Surrey Design Local Distinctiveness Design Guide Householder Extensions and Alterations
Other	Human Rights Act 1998

**6.0 Assessment**

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development and where the principle of such development is acceptable in land use terms.

6.2 The main issues to consider are:

- Impact on local character
- Neighbour amenity
- Highway and parking matters
- Trees and landscaping
- Ecology
- Flooding and Drainage
- CIL
- Affordable housing
- Sustainability and Climate Change
- Other matters

Impact on local character

6.3 The application is for the erection of two 4 bedroom detached dwellings to the rear of nos 59 and 61 Nork Way following the sub-division of the plot along with

off road car parking spaces for 2 cars for each property to the front of the houses and rear garden laid to lawn to the rear. A new access road, adjoining the recently constructed drive to the front of no.61 Nork Way is proposed to be constructed along the boundary with no.63 Nork Way (doctor's surgery).

- 6.4 Policy DES2 refers to development within residential garden land. It is considered that the sub-division of the plots as proposed would comply with this policy as the new houses will respect the scale, form and external materials of the existing buildings in the locality. In addition, the height of the houses and mass will correspond with the others in the street-scene, the elevations provided show that detached houses would be similar in height to other properties within the road. The resultant plot sizes for the donor properties would also be acceptable with plot depths of around 20m.
- 6.5 It is considered that the plot widths are commensurate with other plots in the road. The spacing between the buildings would ensure that houses are in keeping with the prevailing layout of the area including the donor properties. The resultant plot sizes are commensurate or more generous than other back garden development plots such as Willow Close to the rear of the site.
- 6.6 The development will not cause undue disruption to the character and appearance of the existing street frontage as it would utilise the current access to 61 Nork Way and it is noted that the existing access to that property and the driveway has already been constructed. The access road would be set back from the front of the plots and would be largely shielded from view by a substantial tree and planting. The use of access roads is commonly found within Nork Way and it is considered that this access road would be suitable screened by existing and further proposed landscaping in order to soften its appearance.
- 6.7 The proposed dwellings would be of a broadly traditional design typical of the 1920/30s style designs of the other properties in Nork Way. The proposed detached dwellings would have hipped roofs and a part rendered / brick design similar to the donor properties and other houses within Nork Way and the wider area. The plans and design statement show that the proposed dwellings would be constructed out of traditional materials. It is considered important that the external materials are secured by condition to ensure that the materials are in keeping with the surrounding area.
- 6.8 The proposal also includes some enhanced landscaping, details of which can be secured by condition. Close boarded fences are also proposed between the new properties and along the boundaries with the donor properties and this is considered acceptable.
- 6.9 The two dwellings exceed the minimum sizes as stated within the nationally prescribed space standards and complies with policy DES5. The two dwellings would have spacious rear gardens, with depths of these gardens being at least 20m which is generous when assessing against other similar back garden developments.

- 6.10 A new bin store is proposed to the front of the site and details of this will be secured by condition to ensure that its appearance is acceptable in the context of the wider street scene.
- 6.11 It is considered therefore that the quantum of development and the design of the buildings are appropriate on this site and the proposal complies with both policies DES1 and DES2 in this regard.

#### Neighbour amenity

- 6.12 The proposal would increase the bulk and quantum of development on the site. A site visit has been undertaken in order to assess the impact of the new dwellings on neighbouring properties including the donor properties. It is considered that the impact to the property to the north east, at 57 Nork Way is acceptable as the proposed dwellings would be built adjacent to the exceptionally long rear garden, well away from the dwelling and with a good degree of separation to the shared boundary.
- 6.13 The rear gardens of the properties to the north-west, at 15-17 Willow Close, are relatively shallow at around 10m. Conversely, the proposed plots have a relatively deep rear garden of around 20m. The plans show that the back-to-back relationship of the properties are around 30m which is considered acceptable. In addition, the buildings are angled away from each other which would ensure that there would be no significant overlooking impact as any views from first floor rear windows would be oblique.
- 6.14 To the south west are 2 and 4 Warren Road, both of which have relatively long rear gardens and both properties are at a significantly lower level than the application site. No side facing windows at first floor level are proposed and the proposed dwellings are around 30m from these properties which would minimise any significant overlooking or material impact to the amenity of those properties.
- 6.15 The proposed access road would be built between no.63 Nork Way (a doctor's surgery with no residential accommodation) and one of the donor properties at no.61. Whilst there would be some impact to no.61 from the access road, the proposal is for two properties only and the impact especially from noise and disturbance is not considered to cause such harm as to warrant refusal on this ground.
- 6.16 The remaining gardens of both of the donor properties are around 20m in depth and the houses to the rear are at least 28m from those properties. It is considered that any overlooking would be inconsequential and there is anticipated to be robust landscaping along the boundaries between the properties which would minimise any material impact between neighbouring properties.
- 6.17 Consequently, it is considered that the proposal would not cause significant or material harm to the amenity of neighbouring properties and therefore, the proposal complies in this regard with policy DES1.

### Highway and Parking Matters

- 6.18 The County Highways Authority has assessed the proposed development on safety, capacity and policy grounds and have recommended that conditions should be imposed on the permission relating to the new access, parking and turning, and the provision of electrical charging points for cars along with storage and charging for e-bikes and bikes and refuse collection.
- 6.19 DMP policy TAP1 states that new residential development should: "Include car parking and cycle storage for residential and non-residential development in accordance with adopted local standards (see Annex 4). The applicant has stated that each dwelling would include two car parking spaces within the development, and this is considered compliant with policy.
- 6.20 It is noted that there is local concern that this part of the road has historically had issues with on street parking and there are concerns in relation to the loss of the two on street parking spaces. These parking spaces are used by visitors to the local shopping parade to the east. This point has been noted and part of the access condition will require the reinstatement of two on street parking spaces to ensure that there is sufficient parking for the users of the adjacent shopping parade.

### Trees and Landscaping

- 6.21 There is a mature tree to the front of the site adjacent to the access road along with other landscape features of a domestic scale on the proposed plots. The council's tree officer has been consulted and has made the following comments:  
"As the access drive is already in place the key arboricultural feature T3 there will be no further work required that may affect it structurally. The remaining trees are low quality and will need to be removed to facilitate this development as they are located in the rear garden, their removal will not have a noticeable impact on the appearance of the immediate area. Regardless of their stature replacement placement planting is required that will enhance the site and the wider landscape, for this site there is limited scope for this to happen. Despite these concerns from an arboricultural view there is no reason to object to this application."
- 6.22 The loss of landscaping in the rear of the site is considered acceptable as the planting that is there is in the rear gardens is of domestic scale and suitable replanting is proposed. This will be controlled by a landscaping condition that will be added to the permission to ensure that hard and soft landscaping proposed is of a suitable standard and will enhance the character of the area. Subject to these conditions being complied with, it is considered that the proposal complies in this regard with policy NHE3.

### Ecology

- 6.23 The applicants have provided a preliminary ecological assessment. The report states: “No confirmed evidence of bats was located in the roof spaces or in other areas of the site. The property and garage roofs were in good condition and no further bat surveys are considered necessary at this stage, unless there is a significant delay to development, with a follow-up roost assessment needed if development is delayed by several years, or if the mature trees of low bat potential require removal. The only protected species likely to be present on site are birds breeding within scrub and dense sections of hedge, as well as foxes, which are protected from cruelty under the Wild Mammals (Protection) Act 1996.

“In order to ensure that the development delivers biodiversity enhancements, including the provision of species-rich lawns and native hedging, it is recommended that a Landscape Ecological Maintenance Plan (LEMP) is developed through the collaboration of an ecologist and landscape architect. This should include the provision chalk substrate spoil store (during construction), for dressing during landscaping and considerations for laying chalk species-rich lawns and creating several bare-chalk areas to be sown with kidney vetch *Anthyllis vulneraria*, the food-plant for small blue butterfly *Cupido minimus* which breeds. Schedule 9 non-native species (Appendix IV) need to be avoided within landscaping, as well as vigorous non-native shrubs such as cherry laurel. The LEMP should also include the provision of nest boxes and a schedule for protection of breeding birds and foxes.”

- 6.24 It is therefore considered that subject to the provision of a landscape ecological maintenance plan, and this being discharged, the proposal complies with policy NHE2.

### Flooding and drainage

- 6.25 Concerns have been raised from local residents in relation to the potential for flooding and drainage concerns. The site is within flood zone 1 (with the lowest risk of flooding) and no part of the site is within an area of high surface water flooding. It is therefore considered that there is not expected to be a significant flood risk from the proposal and that drainage issues will be assessed by building regulations rather than planning.

### CIL

- 6.26 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, roads, public transport and community facilities which are needed to support new development. This development would be CIL liable although the exact amount would be determined and collected after the grant of planning permission.

### Affordable Housing

- 6.27 Development Management Plan DES6 states that on developments providing 11 or more homes, 30% of the homes on site should provide affordable housing. This supersedes the Core Strategy policy CS15 in its entirety.
- 6.28 In view of this, the Council is not presently requiring financial contributions from applications such as this resulting in a net gain of 10 units or less.

### Sustainability and Climate Change

- 6.29 Policy CCF1 of the Councils Development Management Plan 2019 seeks to ensure that all new development contributes to reducing carbon emissions. New development will be encouraged to incorporate passive and active energy efficiency measure and climate change resilience measures and renewable energy technologies. In order that the proposed development contributes to achieving these aims, in the event that planning permission is granted, a condition requiring demonstration that it will meet the national water efficiency standard of 110litres/person/day will be attached.

### Other Matters

- 6.30 Concern was raised from local residents in terms of potential flooding and drainage issues. It is noted that the site is within flood zone 1 which carries the lowest risk of flooding. In addition, the site is not within an area of high surface water drainage, although it is noted that the road and the junction with Cheyne Walk to the north is in an area of relatively high surface water flooding (1:100). The highways authority have requested a condition to ensure that the new access will be provided with a means to prevent private water from entering the highway and this will help to prevent any additional surface water flooding in the immediate area.
- 6.31 Electronic communication networks: Policy INF3 criteria 1 states that "The Council will require all new development to be connected with high speed and reliable broadband". A condition has been added to the permission to this effect.

## **CONDITIONS**

1. The development hereby permitted shall be carried out in accordance with the following approved plans.

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country

Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Landscaping Plan	O-O-22-30-111	R6	07.11.2023
Detailed Technical Plan	O-O-22-30-113	R2	07.11.2023
Site Layout Plan	O-O-22-30-100	R7	07.11.2023
Floor Plan	O-O-22-30-101	R4	07.11.2023
Proposed Plans	O-O-22-30-102	R1	07.11.2023
Section Plan	O-O-22-30-103	R4	07.11.2023
Site Layout Plan	O-O-22-30-002		07.11.2023
Floor Plan	61-01		07.11.2023
Existing Plans	59-02		07.11.2023
Elevation Plan	59-03		07.11.2023
Elevation Plan	59-04		07.11.2023
Floor Plan	61-01		07.11.2023
Existing Plans	61-02		07.11.2023
Elevation Plan	61-03		07.11.2023
Elevation Plan	61-04		07.11.2023
Location Plan	O-O-22-30-001		07.11.2023
Arb / Tree Protection Plan	SJA TPP 22442-042b 042b		07.11.2023

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004

3. No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

4. No development shall commence including groundworks preparation and demolition until all related arboricultural matters, including arboricultural supervision, monitoring and tree protection measures are implemented in strict accordance with the approved details contained in the Tree Protection Plan compiled by SJA trees reference TPP 22442 – 042 b Rev 0426 , dated October 2023

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction - Recommendations' and policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019.

5. The development shall not be occupied until a scheme for the landscaping of the site including the retention of existing landscape features has been submitted to and approved in writing by the local planning authority. Landscaping schemes shall include details of hard and soft landscaping, including any tree removal/retention, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation and management program.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to first occupation or within the first planting season following completion of the development hereby approved or in accordance with a program agreed in writing with the local planning authority.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with Reigate and Banstead Borough Development Management Plan 2019 policies NHE3, DES1 and DES3, and the recommendations within British Standards including BS8545:2014 and British Standard 5837:2012.

6. Notwithstanding the submitted plan (Drawing No. 0-0-22-30-113 Rev R2) no part of the development shall be first occupied unless and until the proposed vehicular access to 56-61 Nork Way Lane has been constructed with pedestrian sight lines measuring 2 metres by 2 metres and replacement on street parking bays provided in accordance with a revised scheme to be submitted to and approved in writing with the Local Planning Authority and thereafter the visibility zones shall be kept permanently clear of any obstruction over 600mm high to satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

7. The development hereby approved shall not be first occupied unless and until space for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear has been constructed and provided in

accordance with the approved plan (Drawing No. 0-0-22-30-100 Rev R7). Thereafter the parking and turning areas shall be retained and maintained for their designated purposes.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

8. The development hereby approved shall not be occupied unless and until each of the proposed dwellings are provided with a fast charge socket (current minimum requirement: 7kw Mode 3 with Type 2 connector - 230 v AC 32 amp single phase dedicated supply) in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority and thereafter retained and maintained for their designated purpose.

Reason: In order that the development promotes more sustainable forms of transport, and to preserve the character of the Conservation Area, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 and policy TAP1 of the Development Management Plan.

9. The development hereby approved shall not be first occupied unless and until facilities for the secure, covered parking of bicycles and the provision of a charging point with timer for e-bikes by said facilities have been provided within the development site in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority and thereafter the said approved facilities shall be provided, retained and maintained to the satisfaction of the Local Planning Authority.

Reason: In order that the development promotes more sustainable forms of transport, and to preserve the character of the Conservation Area, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 and policy TAP1 of the Development Management Plan.

10. The development hereby approved shall not be first occupied unless and until facilities for refuse collection have been provided in accordance with the approved plan (Drawing No.0-0-22-30-101 rev R4) all to be permanently retained.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

11. No development shall commence until a Landscape and Ecological Management Plan (LEMP) has been submitted to and approved in writing by

the Local Planning Authority (LPA). The LEMP shall be based on the on the submitted preliminary ecological assessment. The LEMP shall include the following:

- a) species specific enhancement features to be installed/created and managed
- b) Ecological trends and constraints on site that might influence management
- c) Aims and objectives of management
- d) Appropriate management options for achieving aims and objectives
- e) Prescriptions for management actions, together with a plan of management compartments
- f) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a thirty-year period)
- g) Details of the body or organisation responsible for implementation of the plan
- h) Ongoing monitoring and remedial measures
- i) Legal and funding mechanisms by which the long-term implementation of the plan will be secured by the applicant with the management body(ies) responsible for its delivery.
- j) Monitoring strategy, including details of how contingencies and/or remedial action will be identified, agreed and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally approved scheme

The agreed details shall be implemented before occupation of this development, unless otherwise agreed in writing by the LPA, and maintained/monitored in accordance with the agreed details.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and Reigate and Banstead Development Management Plan 2019 policy NHE2.

12. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
  - a) A broadband connection accessed directly from the nearest exchange or cabinet,
  - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

13. The development hereby approved shall not be first occupied unless and until a Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day,

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

## INFORMATIVES

1. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

2. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
3. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings. If you are building a scheme of more

than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found [http://www.reigate-banstead.gov.uk/info/20277/street\\_naming\\_and\\_numbering](http://www.reigate-banstead.gov.uk/info/20277/street_naming_and_numbering)

4. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, to contact the Council's Neighbourhood Services team to confirm the number and specification of recycling and refuse bins that are required to be supplied by the developer. All developer enquires on recycling and refuse bin ordering, collections and discussing waste matters is via our department email address RC@reigate-banstead.gov.uk . Please also note our website area for developers [https://www.reigate-banstead.gov.uk/info/20062/recycling\\_and\\_refuse/392/fees\\_for\\_recycling\\_and\\_refuse\\_services/3](https://www.reigate-banstead.gov.uk/info/20062/recycling_and_refuse/392/fees_for_recycling_and_refuse_services/3).
5. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
6. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
7. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
8. In the event that the access works require the felling of a highway tree not being subject to a Tree Preservation Order, and its removal has been permitted through planning permission, or as permitted development, the developer will pay to the County Council as part of its licence application fee compensation for its loss based upon 20% of the tree's CAVAT valuation to compensate for the loss of highway amenity.
9. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Electric Vehicle Charging Points shall be provided in accordance with the Surrey County Council Vehicular, Cycle and Electric Vehicle Parking Guidance for New Development 2022. Where undercover parking areas (multi-storey car parks, basement or undercroft parking) are proposed, the developer and LPA should liaise with Building Control Teams and the Local Fire Service to understand any additional requirements. If an active connection costs on average more than £3600 to install, the developer must provide cabling (defined as a 'cabled route' within the 2022 Building

Regulations) and two formal quotes from the distribution network operator showing this.

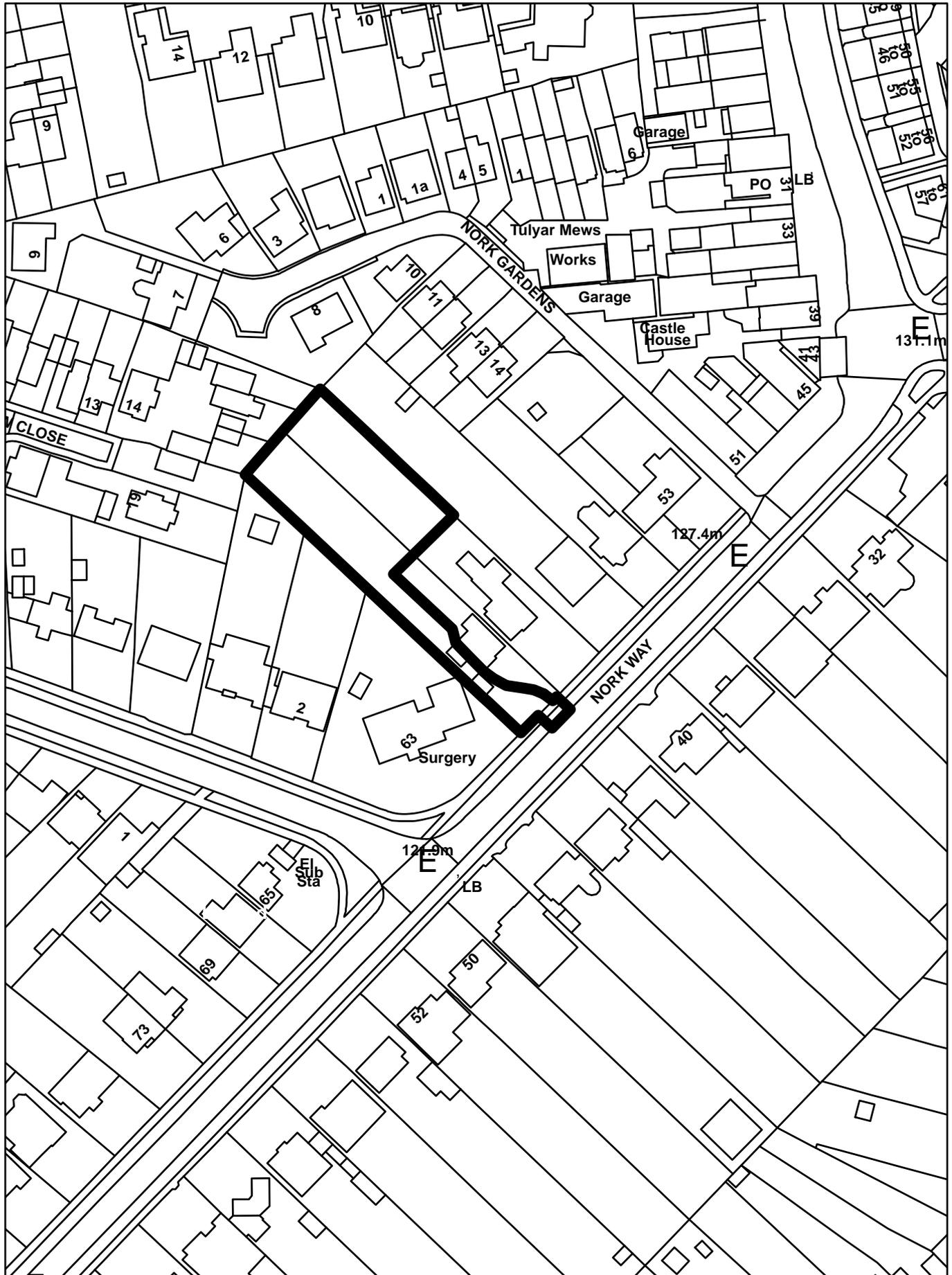
10. The applicant is expected to ensure the safe operation of all construction traffic in order to prevent unnecessary disturbance obstruction and inconvenience to other highway users. Care should be taken to ensure that the waiting, parking, loading and unloading of construction vehicles does not hinder the free flow of any carriageway, footway, bridleway, footpath, cycle route, right of way or private driveway or entrance. Where repeated problems occur the Highway Authority may use available powers under the terms of the Highways Act 1980 to ensure the safe operation of the highway
11. It is the responsibility of the developer to provide e-bike charging points with socket timers to prevent them constantly drawing a current over night or for longer than required. Signage should be considered regarding damaged or shock impacted batteries, indicating that these should not be used/charged. The design of communal bike areas should consider fire spread and there should be detection in areas where charging takes place. With regard to an e-bike socket in a domestic dwelling, the residence should have detection, and an official e-bike charger should be used. Guidance on detection can be found in BS 5839-6 for fire detection and fire alarm systems in both new and existing domestic premises and BS 5839-1 the code of practice for designing, installing, commissioning, and maintaining fire detection and alarm systems in non-domestic buildings.
12. The use of a suitably qualified arboricultural consultant is essential to provide acceptable supervision and monitoring in respect of the arboricultural issues in respect of the above condition. All works shall comply with the recommendations and guidelines contained within British Standard 5837.

## **REASON FOR PERMISSION**

The development hereby permitted has been assessed against development plan policies DES1, DES2, DES5, DES8, TAP1, NHE3, CCF1, INF3, and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

# 23/02289/F - Land To Rear Of 59 & 61 Nork Way, Banstead



**Key**

- 01** - New native hedge to comprise: Acer campestre, Corylus avellana, Crataegus monogyna and Prunus spinosa planted as feathers, 2x, 175-200cm
- 02** - Hedgehog holes 13 x 13cm at bases of fences
- 03** - Permeable driveway
- 04** - Existing mature tree retained with bird and bat boxes added to the trunks
- 05** - New acer campestre EHS 14-16cm
- 06** - New Ilex aquifolium standard 8-10cm
- 07** - New Crataegus monogyna 8-10cm
- 08** - New Prunus avium robor EHS 12-14cm
- 09** - New Corylus avellana EHS 12-14cm
- 10** - New Large Mature Street Tree - Norwegian Maple
- 11** - New perennial grass and shrub beds, mixed species tbc.

**Soft Landscape Specification Notes**

General: All soft landscape works are to be carried out in accordance with the latest editions of: BS 5837, BS 4428 and BS 7370-4. The existing soil and any imported soil required for the completion of the works is to be subject to a topsoil analysis and the report with any required amelioration to be carried out.

Topsail depths: Shrub and hedge planting 450mm. Lawns 150mm.

General: All plant stock to conform to the standards set out in the National Plant Specification, to be sturdy, healthy and vigorous. Of good shape and without elongated shoots, grown in a suitable environment and hardened off, free from pests, diseases, discoloration, weeds and physiological disorders, have a balanced branch system and be true to name.

Plant Handling, Storage and Transportation: To accord with HTA "Handling and establishing landscape plants" Preparation of Planting Beds:  
Prior to planting all topsoil to be dug over to a depth of 150mm within 2-3 days prior to planting when weather and ground is suitably dry.  
Leave to regular and even levels: 25mm above adjoining paving or kerbs.  
Remove all visible weeds and large stones exceeding 50mm.

Tree Planting  
Trees to be planted in prepared pits 1200 x 1200 x 900mm deep. The pit bottom is to be broken up to a depth of 150mm and to be planted at the same depth as grown on the nursery with approved tree backfilling material and well consolidated. Planting in grass areas are to be provided with a 1200mm diameter area clear of turf and weeds.

Immediate after planting, all trees to be supported with a short double stakes and cross bar. Stakes to be peeled chestnut with pointed lower end. Stakes to be finished at 600mm above ground level and fitted with cross bar to match stake.  
To be secured on windward side of bar with cushioned tree ties secured firmly to bar.

Mulching: All trees to be mulched after planting with Ornamental Bark Mulch (Melcourt or equal approved) to a depth of 75mm to form an area 1200 x 1200mm. Immediately after planting water all trees to soil field capacity.

Shrub and Hedge Planting  
To be planted in pits 150mm wider and deeper than container or roots when fully spread. All pit bottoms to be broken up to a depth of 150mm, incorporating soil ameliorant at 50g/m2 and well consolidated.  
Mulching: All shrub planted areas to be mulched with Ornamental Grade Mulch (Melcourt, or equal approved) to a depth of 75mm and watered to soil field capacity.

Turfing  
Prior to laying turf or seeding all areas to be reduce to a fine, firm tilth with good crumb structure. Raked to a true even surface, friable and lightly firmed but not compacted  
All lawn areas to be laid with approved cultivated turf within 18 hours of delivery. Avoid laying in persistent cold, or drying winds, or when the soil is frost bound.  
Turf to be laid off planks with broken joints well butted up.  
All lawns to be watered to soil field capacity immediately after laying

**General notes**

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Status R:	Revision	Date	DFN	CHK	CDM
	1	08/08/2022			OO
	2	30/09/2022			OO
	3	09/11/2022			OO
	4	12/12/2022			OO
	5	27/04/2023			OO
	6	12/09/2023			OO

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	Client approval	<input type="checkbox"/>
	Construction	<input type="checkbox"/>
	Final Issue	<input type="checkbox"/>

Client:

Trinity Square Developments

Project title:

59/61 Nork Way

Drawing title:

Proposed Landscape Plan

Scale @ A3 size

1:500

Date

August 2022

Drawing N°:

O-O-22-30-111

Status & Revision

R6

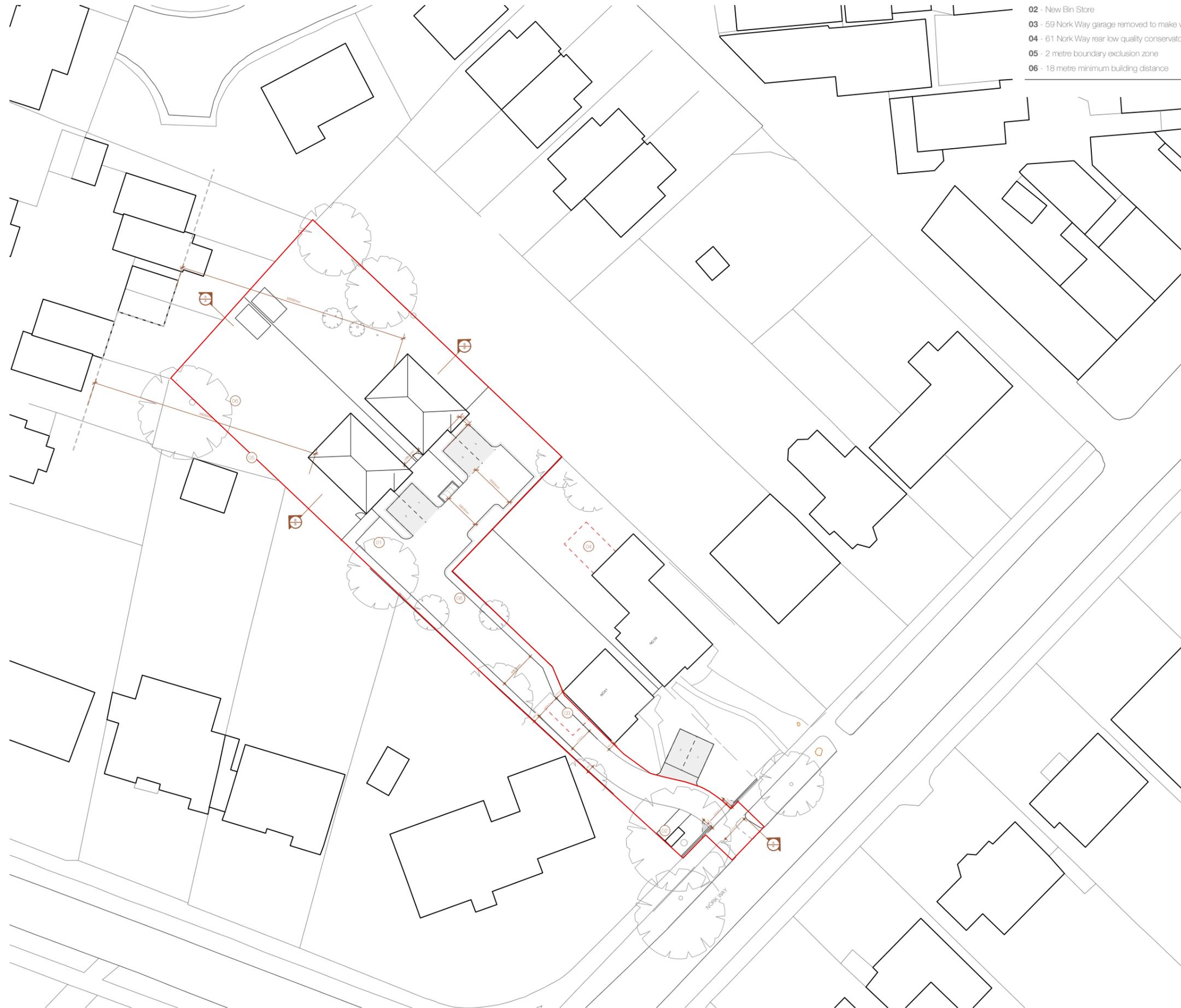
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**Key**

- 01 - Turning Head
- 02 - New Bin Store
- 03 - 59 Nork Way garage removed to make way for new access road
- 04 - 61 Nork Way rear low quality conservatory removed
- 05 - 2 metre boundary exclusion zone
- 06 - 18 metre minimum building distance



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	2	08/06/2022			OO
	3	16/06/2022			OO
	4	13/07/2022			OO
	5	28/09/2022			OO
	6	12/12/2022			OO
	7	12/09/2023			OO

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	Construction	<input type="checkbox"/>
	Final Issue	<input type="checkbox"/>

Client

Trinity Square Developments

Project title

59/61 Nork Way

Drawing title

Proposed Site Plan

Scale @ A3 size

1:500

Date

May 2022

Drawing N°

O-O-22-30-100

Status & Revision

R7

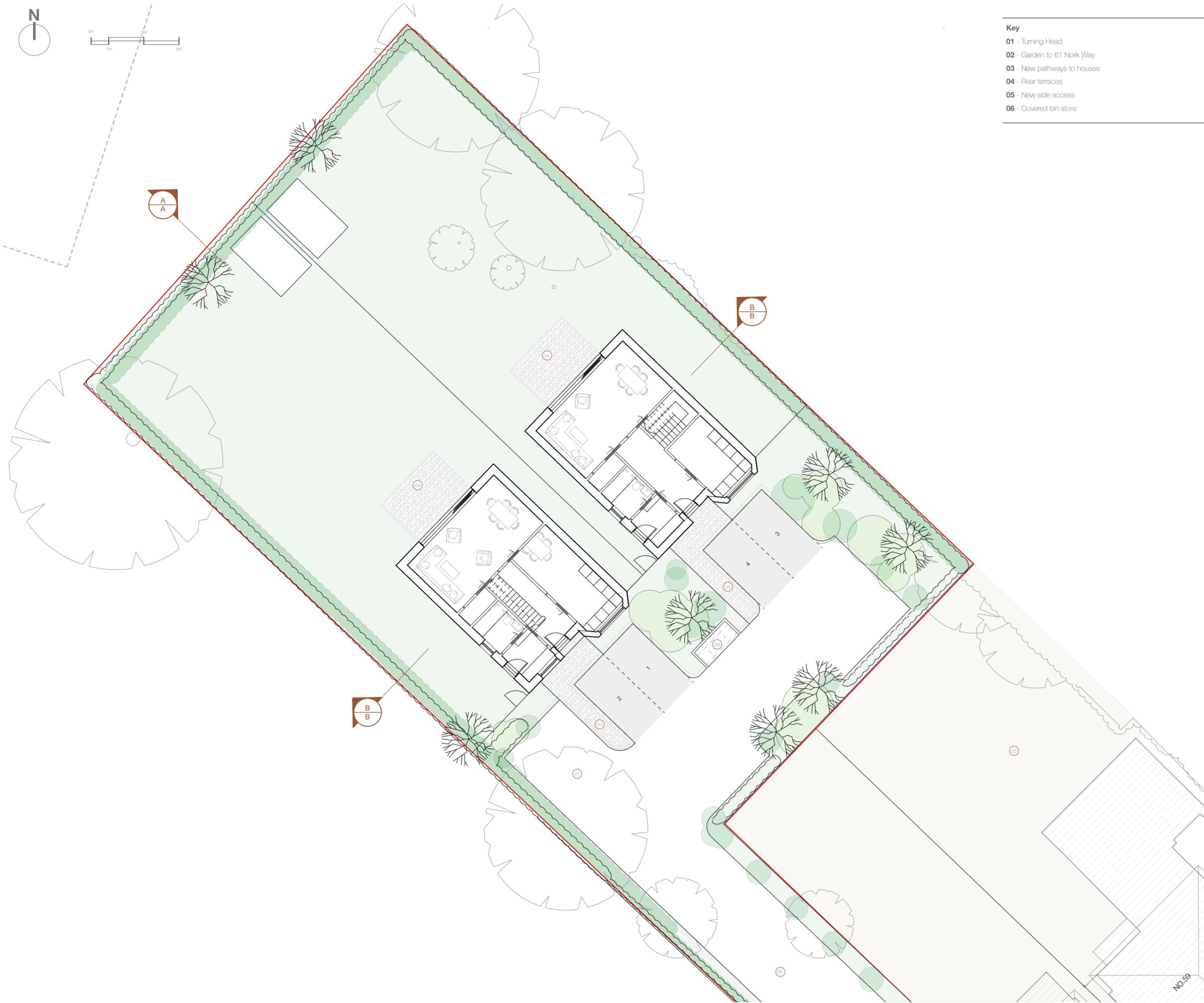
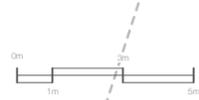
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 1142200402100

Steve Olive  
 Director  
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 1144200402100





**Key**

- 01 - Turning Head
- 02 - Garden to 61 Nork Way
- 03 - New pathways to houses
- 04 - Rear terraces
- 05 - New side access
- 06 - Covered bin store

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4		12/09/2023			OO

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	Construction	<input type="checkbox"/>
	Final Issue	<input type="checkbox"/>

Client

Trinity Square Developments

Project title

59/61 Nork Way

Drawing title

Ground Floor Plan

Scale @ A3 size

1:200

Date

Apr' 2021

Drawing N°

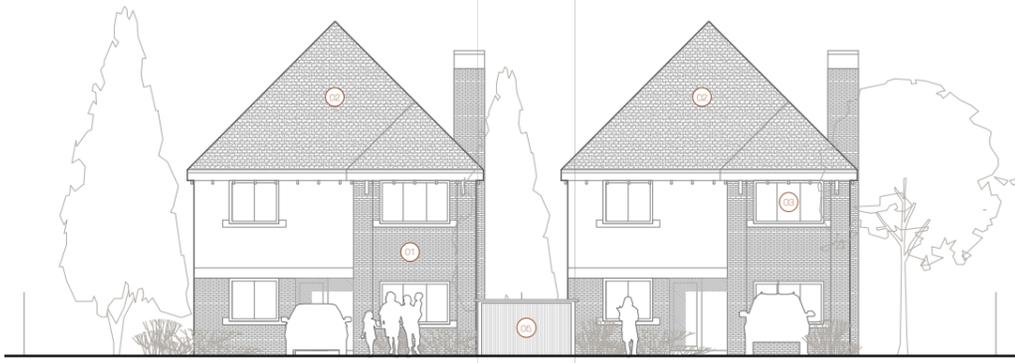
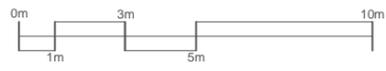
O-O-22-30-101

Status & Revision

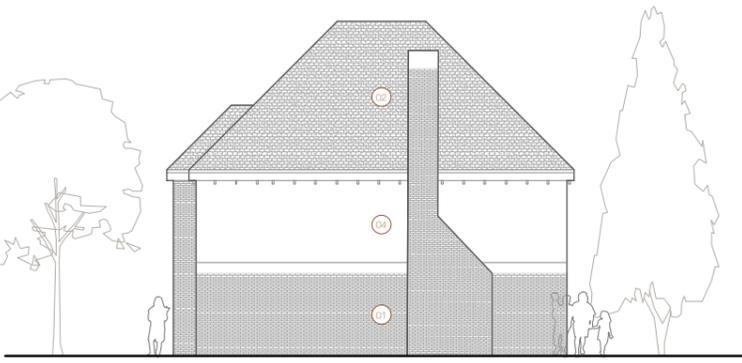
R4

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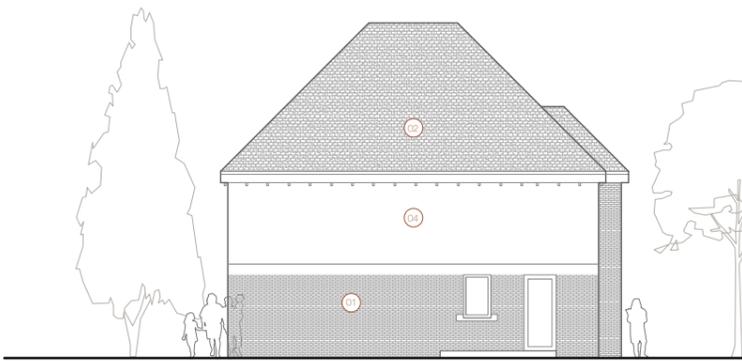
Proposed South Facing Elevation



Proposed East Facing Elevation



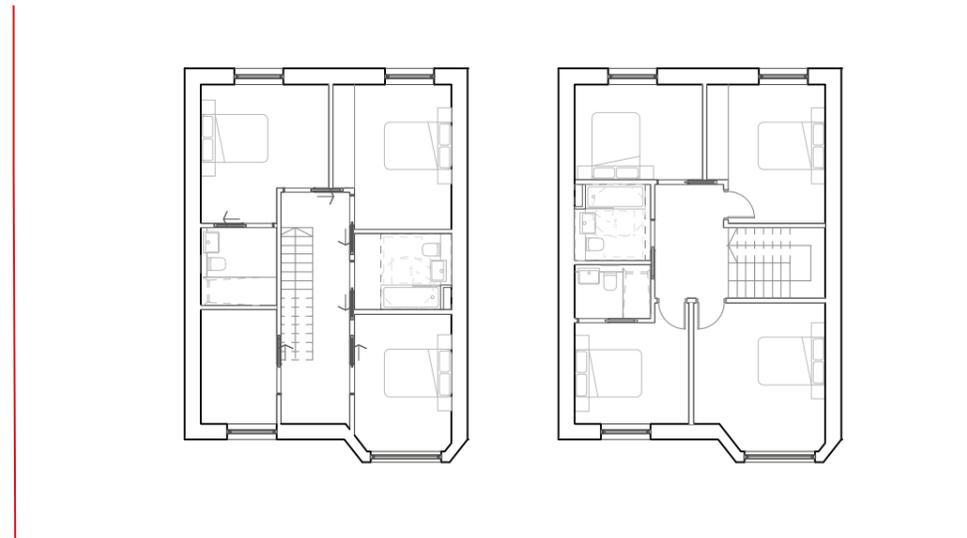
Proposed North Facing Elevation



Proposed West Facing Elevation



Proposed Ground Floor Plan



Proposed First Floor Plan

**Key**

- 01 - Brickwork
- 02 - Red Tile Roof
- 03 - Timber Composite Window
- 04 - Render Finish
- 05 - Vertical Timber Finish

**General notes**

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	1	08/06/2022			OO

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	Construction	<input type="checkbox"/>
	Final Issue	<input type="checkbox"/>

Client

Trinity Square Developments

Project title

59/61 Nork Way

Drawing title

Existing Elevations

Scale @ A3 size

Date

1:200

June 2022

Drawing N°

O-O-22-30-102

Status & Revision

R1

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