 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	23 rd January 2019
	REPORT OF:	HEAD OF PLACES & PLANNING
	AUTHOR:	Hollie Marshall
	TELEPHONE:	01737 276010
	EMAIL:	Hollie.marshall@reigate-banstead.gov.uk
AGENDA ITEM:	5	WARD: Nork

APPLICATION NUMBER:		18/01920/F	VALID:	19th September 2018
APPLICANT:	The Harnel Group C/o Two County Homes Ltd		AGENT:	Colin Smith Planning Ltd
LOCATION:	LAND REAR OF 127-139 RUDEN WAY EPSOM DOWNS SURREY KT17 3LW			
DESCRIPTION:	Demolition of nos 129 and 131 Ruden Way and the erection of 6 new detached dwellings with access from Ruden Way together with car parking and landscaping. As amended on 06/12/2018.			
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.				

SUMMARY

This is a full application for demolition of no's 129 and 131 Ruden Way and the erection of 6 new detached dwellings on land to the rear of 127 to 139 Ruden Way, with access from Ruden Way together with car parking and landscaping. During the course of the application the proposal has been reduced from seven to six proposed dwellings, and these would be arranged in a linear layout to the rear of the donor properties.

The proposal follows a recent application for the redevelopment of the site (18/00548/F), which was refused and is currently at appeal. Whilst no in principle objection was raised to the redevelopment of the site, given the particularly long rear gardens of the donor properties and the Core Strategy's acknowledgement of the development potential of such sites, the application was refused due to the proposal resulting in a cramped and visually dominant overdevelopment of the site and concern in regard to the amenity of future occupants by way of noise and disturbance. Though the land is designated as Green Belt beyond the railway line to which the site backs on, it was not argued previously that there would be harm to the transition to this given the developments nearby that are located much closer to this green belt boundary.

This current scheme for 6 dwellings is considered to have overcome the particular concerns identified with the previous application for 9 and is considered acceptable.

In particular the following changes have been made to overcome the three main concerns identified:

- The reduced number of dwellings creates a more spacious layout with greater separation distances between dwellings and the donor properties. There are greater levels of visual separation and increased areas of soft landscaping.
- The linear layout allows for increased rear garden depths and increased plot sizes.
- The revised design approach uses hipped roofs with a reduced ridge and eaves height, significantly reducing the bulk and mass of the dwellings.

These changes are considered to significantly improve the scheme from that recently refused. As a result the proposal appears more spacious.

With regard to the amenity of future occupants, given the site backs onto a railway line, the application is accompanied by a noise report. The report concludes that subject to appropriate mitigation measures, the site is suitable for residential development in terms of noise. This is considered to be accurate, especially given the branch line nature of the line in question. A condition is recommended to require no development shall take place until a scheme for noise mitigation measures for the dwellings has been submitted to and approved in writing by the local planning authority.

The proposal is not considered to result in a harmful impact upon neighbour amenity and the County Highways Team have confirmed no objection subject to recommended conditions. There are no significant trees on the site that would be affected and the proposal is therefore considered acceptable in all regards.

RECOMMENDATION

Planning permission is **GRANTED** subject to conditions.

Consultations:

Highway Authority: The County Highway Authority has undertaken an assessment in terms of the likely net additional traffic generation, access arrangements and parking provision and are satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway. The County Highway Authority therefore has no highway requirements subject to conditions.

Thames Water – no objection subject to condition and informatives

Network Rail – no comments received

Contaminated Land Officer – no comments received

Environmental Health – no comments received

Neighbourhood Services – The developer will need to provide a presentation point where residents can place their bins for collection.

Sutton and East Surrey Water Company – no comments received

Nork Residents Association – objection raised on the grounds of harm to green belt, cramped, inadequate parking, hazard to highway safety, car dominated, out of character with surrounding area, overbearing, overlooking, overdevelopment, increase in traffic and congestion, development of greenfield site.

Representations:

Letters were sent to neighbouring properties on 21st September 2018 and again on 19th December 2018 following an amendment to the site layout a site notice was posted 8th October 2018

19 responses have been received raising the following issues:

Issue	Response
Poor design	See paragraph 6.7 and condition 4
Out of character with surrounding area	See paragraph 6.3 – 6.10
Cramped	See paragraph 6.3 – 6.6
Overbearing relationship	See paragraph 6.12
Overlooking and loss of privacy	See paragraph 6.12
Harm to wildlife habitat	See paragraph 6.26 – 6.27, condition 14
Inadequate parking	See paragraph
Increase in traffic and congestion	See paragraph 6.21 – 6.23
Overdevelopment	See paragraph 6.3 – 6.10

Hazard to highway safety	See paragraph 6.21 – 6.23 and conditions 7, 8 and 9
Inconvenience during construction	See paragraph 6.30 and condition 5
Loss of/harm to trees	See paragraph 6.20 and
No need for the development	See paragraph 6.1
Noise and disturbance	See paragraph 6.13, 6.15 – 6.19, 6.31 and condition
Light pollution	See paragraph 6.32
Air pollution	See paragraph 6.32
Conflict with a covenant	See paragraph 6.34
Drainage/sewage capacity	See paragraph 6.28 and condition 10
Impact on existing utilities	See condition 10
Impact on infrastructure	See paragraph 6.25
Fire Service access	See paragraph 6.29
Loss of a private view	See paragraph 6.34
Affordable housing	See paragraph 6.24
Overshadowing	See paragraph 6.11
Contrary to Human Rights	See paragraph 6.31
Amenity of future occupants	See paragraph 6.15 – 6.19 and condition 11
Harm to Green Belt/countryside	See paragraph 6.9
No need for the development	See paragraph 6.1
Harm to Conservation Area	See paragraph 6.9
Property devaluation	See paragraph 6.34
Back garden land development	See paragraph 6.1 and 6.3

1.0 Site and Character Appraisal

- 1.1 The application site is located on the western side of Ruden Way, and comprises No. 129 and 131 Ruden Way and the rear gardens of 127, 133, 135, 137 and 139 Ruden Way. Ruden Way is residential road, which leads off Reigate Road at the north eastern end and Fir Tree Road at the southern end. Three residential developments have been built off of Ruden Way in more recent years – Amber Close, Poplar Close and Whitebeam Close.
- 1.2 The character of this part of the road where the application site lies within is predominantly semi-detached houses, set in elongated plots. The dwellings are largely modest in scale, with mainly mansard style roofs in part of Ruden Way. They are of a similar scale with spacious gaps between. The dwellings are set back from the road on a uniform building line, following the curve of the road, with green verges with street trees and pavements. The dwellings

towards the south end of the road are predominantly detached and the plots increase in width creating a more spacious feel. At the northern end of the road lies a large commercial site in use as a car garage.

- 1.3 The Council's Local Distinctiveness Design Guide identifies the area as one of 1930s-1950s Suburbia. The road increases in level to the south. The site also increases in gradient towards the rear (west) where a railway line borders the application site. Gardens to the rear of the site are well established with some large amenity trees and mature boundary vegetation.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: Pre-application advice was sought and concern raised over the proposed layout, in particular the isolated plot in the central part of the site.
- 2.2 Improvements secured during the course of the application: During the course of the application amendments were sought to reduce the layout from seven to six dwellings and design amendments have been sought to the dormer designs and materials.

3.0 Relevant Planning and Enforcement History

- 3.1 18/00548/F Demolition of 129 and 131 Ruden Way and the erection of 9 new detached dwellings with access from Ruden Way together with car parking and landscaping. Refused 9th May 2018
Appeal in progress
- 3.2 There is also planning history for householder extensions at 127, 129, 131 and 135 Ruden Way.

4.0 Proposal and Design Approach

- 4.1 This is a full application for demolition of no's 129 and 131 Ruden Way and the erection of 6 new detached dwellings on land to the rear of 127 to 139 Ruden Way, with access from Ruden Way together with car parking and landscaping.
- 4.2 The proposed dwellings would be of two similar designs to one another and would comprise of 6 x five bedroom houses. All dwellings would be detached and would be two storeys plus accommodation in the roof space. Plots 1 and 6 would be a mirrored design of one another and would include integral garages. The houses would have a hipped roof with front and rear facing gable features. These plots would include side and rear facing dormer windows to serve the roof space accommodation. Plots two to five would all be of the same design however plots two and four would be a mirrored design of three and five. These plots would be slightly narrower and shallower in dimension but of a similar design with again hipped roofs with front and rear facing gable projections and side and rear facing dormer windows. Two

tandem parking spaces are proposed to the front of plots two to five and one parking space to the front of plots one and six. The proposed finishing materials would include a pallet of red/brown rustic bricks and sandfaced plain roof tiles.

- 4.3 The dwellings would have a separation distance of 2m between each of them with gaps of 1.3m to either side boundary at each end of the site. The dwellings would be served by a new access road that would be built in replacement of the existing pair of semi-detached houses 129 and 131 Ruden Way. The access road would include areas for landscaping to either side as it enters the site and towards the rear part of the site would curve north eastwards towards plot six. The layout would include an area of landscaping directly to the rear of the donor properties ranging in depth between 4.8 and 7.5m. An area of landscaping would also be included at either end of the access road.
- 4.4 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.
- 4.5 Evidence of the applicant's design approach is set out below:

Assessment	The surrounding area is assessed as being characterised by semi detached dwellings, many with mansard type roofs, but others with hipped and gable end roofs. Properties have off street parking and Ruden Way itself has grass verges with some street trees separating the footway from the road. A key characteristic of the area is the number of developments that have been granted planning permission in recent years for the development of back garden areas.
	No site features worthy of retention were identified.
Involvement	No community consultation took place.
Evaluation	The statement does include additional proposed site layouts being considered.
Design	The applicant's reasons for choosing the proposal from the available options were informed by other similar developments in the locality and pre-application advice.

4.6 Further details of the development are as follows:

Site area	0.3 hectares
Proposed parking spaces	12
Parking standard	12 (maximum)
Net increase in dwellings	6
Dwellings per hectare	
Existing site density	15 – 127 to 139 Ruden Way
Proposed site density	26 – proposed application site
Density of the surrounding area	31 – Amber Close

5.0 Policy Context

5.1 Designation

Urban area

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development
CS4 (Valued Townscapes and Historic Environment)
CS10 (Sustainable Development),
CS14 (Housing Needs)
CS15 (Affordable Housing)

5.3 Reigate & Banstead Borough Local Plan 2005

Landscape & Nature Conservation	Pc2G , Pc4
Housing	Ho9, Ho13, Ho14, Ho16
Movement	Mo5, Mo7
Utilities	Ut3

5.4 Other Material Considerations

National Planning Policy Framework	
National Planning Practice Guidance	
Supplementary Planning Guidance	Surrey Design Local Distinctiveness Design Guide A Parking Strategy for Surrey Parking Standards for Development Affordable Housing
Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010

6.0 Assessment

6.1 The application site is within the urban area, where there is a presumption in favour of sustainable development and where the principle of residential development is acceptable.

6.2 The main issues to consider are:

- Design appraisal
- Neighbour amenity
- Amenity for future occupants
- Impact on trees
- Access and parking
- Affordable Housing and Community Infrastructure Levy
- Ecology
- Other matters

Design appraisal

6.3 The proposed development would result in the demolition of No. 129 and 131 Ruden Way and the erection of 6 dwellings to the rear of 127 to 139 Ruden Way. The proposal follows a recent application for the redevelopment of the site for nine dwellings (18/00548/F), which was refused and is currently at appeal. The application was refused due to the development, by virtue of the scale, height, massing and design of the dwellings coupled with the tight layout with excessive areas of hardstanding and lack of opportunity for meaningful landscaping, result in a cramped and visually dominant overdevelopment of the site which would be out of keeping with, and significantly harmful to the character and appearance of the area. No objection was or is raised to the principle of development, given the extremely long rear gardens in which the development would site, the recent examples of similar schemes in the vicinity and the acknowledgement within the Council's Core Strategy of the role such sites should play in housing delivery.

6.4 The current proposal seeks to overcome the recent previous concerns by way of a revised layout and reduction in the number of units from the nine previously proposed to six. The application was originally submitted with a total of seven new dwellings proposed, however during the course of the application following dialogue with the applicant whereby Officers expressed concern over the proposed layout resulting in a cramped form of development, has been reduced to six new dwellings. The dwelling omitted occupied a more isolated position, directly to the rear of the donor properties in the central part of the site.

6.5 The proposed dwellings would be arranged in a linear form, parallel to the dwellings fronting Ruden Way to the front and the railway line to the rear. The reduction in the total number of dwellings allows for the creation of a well spaced landscaped access road and buffer area of landscaping between the rear of the donor properties and the frontages of the new

dwellings in the public areas of the site. This revised layout would create greater visual separation between the existing dwellings and the proposed new houses. Furthermore there has been a reduction in the ridge heights of 1.1m from that of the previously proposed tallest houses to that of the currently proposed, and reduction in eaves height of 0.7m, resulting in a significant decrease in the bulk and mass of the proposed dwellings. As a result the proposal appears more spacious with generous distances to site boundaries and is considered to overcome the earlier concerns in this regard and the previous reason for refusal.

- 6.6 The dwellings would occupy a higher land level than the frontage dwellings along Ruden Way; however their reduced ridge and eaves height results in a scale of development that would better follow with the character of area without resulting in dominating form of development. The proposed area of landscaping to the rear of the donor properties would allow for new planting in this area that would provide some screening from views between the gaps of the existing dwellings fronting Ruden Way. This would increase over time as the planting matures. The revised layout with a reduced number of dwellings, increased visual separation allowing for greater areas of landscaping and reduced bulk and mass would avoid a harmful impact upon the character of the streetscene and result in a development that would integrate with the character of the area.
- 6.7 The design of the dwellings would include hipped roofs, and front and rear facing gable features. Traditional roof forms are characteristic of the area and this design would follow these features. Plots one and six would include a catslide roof to the front elevation adding visual interest. During the course of the application concern was raised over the proposed pallet of materials and plans were amended to propose a red/brown rustic brick and a sandfaced plain roof tile. This softer pallette of materials would also avoid a contrasting appearance to the development and the exact specification of the materials would be secured by way of condition.
- 6.8 In terms of plot sizes, the reduction in the number of units allows for rear garden depths of between 13.5m and 15.9m. The garden depths allow for significantly more generous plot sizes to that of the earlier application, bringing the rear elevations of the proposed dwellings further from the rear boundary with the railway line. To avoid overdevelopment of the plots, conditions would be attached to remove permitted development rights for extensions and new windows, including dormers.
- 6.9 The site is not within nor adjacent to a Conservation Area, Green Belt designation and is not considered to cause harm in this regard. Land beyond the railway line to the rear of the site is within the green belt designation, however the proposal is considered significantly spaced so as to create an acceptable transition to the green belt land beyond the railway line. Furthermore, there are examples of development much closer to this green belt boundary at Bunbury Way to the south and commercial works of the north. The losses of the existing dwellings are not considered detrimental to the character of the locality.

- 6.10 The traditional design of the dwellings is considered to integrate well with the character of the locality, which has a variance of dwelling types and styles. The proposed development is considered to overcome the concerns of the recently refused application. As such, it would cause no harm to the character of the area and would comply with policies Ho9, Ho13, Ho14 and Ho16 of the local plan.

Neighbour amenity

- 6.11 The proposed development has been assessed with regards to its impact on the amenity of neighbouring properties. Objections have been expressed about the impact of the proposal on local residential amenities, as regards overdevelopment, overbearing effect and impacts with respect to overshadowing, overlooking and loss of privacy
- 6.12 Given the separation distances between the new houses and existing residential dwellings on Ruden of between approximately 42m to 46m, it is not considered that the proposed buildings would cause such a level of harm to neighbour amenity in terms of overbearing, overlooking, loss of light or loss of outlook so as to warrant refusal. The new dwellings would be located adjacent to the rearmost garden areas of these neighbours, which are typically less well used and protected than other parts.
- 6.13 The development proposes a new access to replace No. 129 and 131. The access would be sited relatively centrally at the entrance point before curving along the access road and then angling towards the north and into the proposed development. The separation distance created and opportunity for landscaping is considered acceptable along the side boundaries of 127 and 133 Ruden Way and to the rear of the donor dwellings and is considered this would not give rise to undue noise and disturbance resulting in a harmful impact upon the amenities of this dwelling and their enjoyment of their garden area and this element of the proposal is considered acceptable.
- 6.14 While giving rise to a degree of change in the relationship between buildings, the proposed scheme would not unacceptably affect the amenity of neighbouring properties, and complies with policy Ho9.

Amenity for future occupants

- 6.15 To the west of this boundary lies a railway line that serves the end of the line Epsom Downs station sited approximately 200m south west of the application site. A second reason for refusal raised the issue of the shallow plots and close proximity of the adjacent railway line would - in absence of further evidence - result in an unsatisfactory level of amenity through the potential for significant noise and disturbance.
- 6.16 The revised layout allows for rear gardens with depths of between 13.5m and 15.9m. This is an increase from the previously proposed average garden depth of 6.5m.

- 6.17 This application was supported by a noise report that contains a noise survey which was conducted between 16:00 on Monday 20th August to 13:00 on Tuesday 21st August. The results show noise levels consistently peaked over 65db, and on 28 occasions over 70db between the hours of 16:00 and 00:00 and between 07:00 and 13:00. The survey does not cover a full 24 hour period, but given the pattern shown over the hours surveyed, it is considered reasonable to assume that the occurrences of levels over 70db would be higher than those identified in the survey in a full 24 hour period as the period not surveyed was the middle of the afternoon when trains would be running.
- 6.18 In section 10 of the report, the predicted worse case internal noise results are shown with windows closed. The report notes that 'the predicted worst case internal noise levels with windows partially opened exceed the proposed target levels (as is often the case).' It is commonplace for windows to be opened to control ventilation and temperature and residents value the ability to open windows at will at any time of the day, night and year, more commonly in summer but also in the winter months also.
- 6.19 The report concludes that subject to appropriate mitigation measures, the site is suitable for residential development in terms of noise. A condition is recommended to require no development shall take place until a scheme for noise mitigation measures for the dwellings has been submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details before the dwellings hereby permitted are first occupied. This condition is considered necessary and reasonable to ensure a satisfactory living environment for future occupants.

Impact on trees

- 6.20 The submitted plans show a number of trees to be removed from the existing rear gardens. The Tree Officer was consulted upon the proposal and noted the trees within the gardens are low value specimens and their removal will not have a noticeable impact on the character of the local area. The revised layout reducing the proposal to six dwellings will allow a meaningful landscape scheme to be implemented which overtime will add value to the local area. The arboricultural report refers to a previous layout and therefore it will be necessary for a revised report to be submitted, this can be secured by way of suitably worded tree protection and landscaping conditions.

Highway matters

- 6.21 The County Highway Authority (CHA) has undertaken an assessment in terms of the likely net additional traffic generation, access arrangements and parking provision and are satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway. The County Highway Authority therefore has no highway requirements subject to conditions.

- 6.22 The proposed conditions would include the requirement for a construction transport management plan to be submitted prior to commencement of development. Also a condition is recommended that secures the requirement, prior to occupation, for the proposed access to be constructed in accordance with a scheme to be submitted and approved in writing by the Local Planning Authority. A third condition is recommended to secure the closure of the existing accesses from Ruden Way.
- 6.23 The CHA also note the developer is proposing adequate car parking space in accordance with Reigate and Banstead Parking Standards. The developer is also proposing adequate turning space within the site for refuse vehicles to enter and leave the site in forward gear with the Council's Neighbourhood Service team confirming this would be acceptable.

Affordable Housing and Community Infrastructure Levy

- 6.24 The development would result in a net gain of four residential units. Core Strategy Policy CS15 and the Council's Affordable Housing SPD require financial contributions towards affordable housing to be provided on housing developments of 1-9 units. However, the 2018 NPPF exempts developments of 10 units or less from making any affordable housing contributions. The absence of an agreed undertaking does not therefore warrant a reason for refusal in this case.
- 6.25 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, roads, public transport and community facilities which are needed to support new development. This development would be CIL liable although, the exact amount would be determined and collected after a grant of planning permission. However, an informal assessment would indicate a contribution of around £95,200 being required.

Ecology

- 6.26 Concern has been raised regarding the potential for harm to wildlife. Whilst the proposal would result in the redevelopment of rear gardens, it is not considered likely to result in significant impact on existing wildlife habitats. Bats, their roosts and nesting birds are protected by law, and the protected species legislation applies independently of planning permission. Notwithstanding this, objections have been submitted by neighbouring properties indicating the presence of bats on the site and within neighbouring gardens. In the absence of survey evidence from the applicant confirming the presence or absence of protected species on the site and an appropriate mitigation strategy should surveys indicate this necessary, a condition is recommended to require this information prior to the commencement of development.
- 6.27 With regard to boundary treatments, secured by recommended condition 11 the developer is encouraged to incorporate measures to promote

biodiversity and wildlife and to allow wildlife to move into and out of gardens, such as hedgehog friendly gravel boards, where appropriate. Details of the 'wildlife friendly' measures should be identified within the submission of the details for approval and an informative to this effect would be added to the decision.

Other matters

- 6.28 Concern has been raised from neighbouring properties regarding flooding and drainage/sewage. The site is located within flood zone 1 and sewage capacity would be assessed at building control stage. Thames Water has provided advisory information and a suitably worded informative is proposed in regard to the disposal of foul and surface water drainage.
- 6.29 Objection was raised on the grounds of access for a fire service appliance. It is usual for fire appliances to access up to a maximum distance of 45m from an external door for normal low-rise housing. If a fire appliance needs to enter a private drive to get within this 45m distance, a 3.7m wide driveway corridor will normally be sufficient. In this instance the proposed access road is 4.5m in width with a turning head. This issue would be addressed at Building Regulations stage.
- 6.30 Objection was raised on the grounds of inconvenience during the construction period. Whilst it is acknowledged there may be a degree of disruption during the construction phase, the proposal would not warrant refusal on this basis and statutory nuisance legislation exists to control any significant disturbance caused during the construction of the proposal. A construction method statement would be secured by planning condition.
- 6.31 The proposed development would be in residential use, and the proposal is not considered to result in a harmful impact in terms of noise and disturbance to neighbouring dwellings. Regard has been had to the Human Rights Act 1998. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.
- 6.32 The proposal would result in the redevelopment of rear gardens, new boundary treatment is proposed and the development is not considered to cause crime issues. No significant health issues are considered to arise as a result of the planning application. Given the scale of the proposed development and residential nature, the proposal is not considered to result in a harmful impact in regard to air or light pollution or nuisance from headlights.
- 6.33 During the course of the recent application (18/00548/F) a neighbour raised objection on the grounds of the possibility contaminated land and the presence of unexploded ordnance (UXO) on site. The Contaminated Land Officer was consulted upon the proposal recommended an informative in regards to potential for the land to be contaminated by virtue of the previous historical use of the land. With regard to unexploded ordnance, the

Contaminated Land Officer noted that no records are held to corroborate this; however, an informative would be appropriate to raise awareness and clarify the processes should UXO be encountered. Neither of these issues are considered to prevent development of the site subject to safe working practices.

- 6.34 Conflicting with a covenant, loss of a private view and property devaluation are not material planning considerations.

CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Elevation Plan	RW_PA2 011		12.09.2018
Elevation Plan	RW_PA2 012		12.09.2018
Floor Plan	RW_PA2 003		19.09.2018
Location Plan	RW_PA2 001	A	19.09.2018
Site Layout Plan	RW_PA2 002	A	19.09.2018
Elevation Plan	RW_PA2 004		19.09.2018
Section Plan	RW_PA2 103	A	08.01.2019
Site Layout Plan	RW_PA3 100	A	08.01.2019
Floor Plan	RW_PA2 101	A	08.01.2019
Elevation Plan	RW_PA2 102	B	08.01.2019
Floor Plan	RW_PA2 201	A	08.01.2019
Elevation Plan	RW_PA2 202	B	08.01.2019
Section Plan	RW_PA2 203	A	08.01.2019
Site Layout Plan	RW_PA3 100	A	06.12.2018

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason:

To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Borough Local Plan 2005 policy Ho9.

4. No development above ground floor slab level shall take place until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Ho13.

5. No development shall commence including groundworks or demolition until a detailed Tree Protection Plan (TPP) shall be submitted to and approved in writing by the local planning authority. The TPP shall contain details of the specification and location of tree protection (barriers and/or ground protection) and any construction activity that may take place within the protected root areas of trees/hedges shown, where retained on the TPP. The tree protection measures shall be installed prior to any development works and will remain in place for the duration of all construction works. The tree protection barriers/ground protection shall only be removed on the completion of all construction activity, including hard landscaping. All works shall be carried out in strict accordance with these details when approved.

Reason:

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies Pc4 and Pc12 of the Reigate and Banstead Borough Local Plan.

6. No development shall commence on site until a scheme for the landscaping of the site including the retention of existing landscape features has been submitted to and approved in writing by the LPA. Landscaping schemes shall include details of hard and soft landscaping, including any tree removal/retention, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation and management programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or within the first planting season following completion of the development hereby approved.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, shrubs of the same size and species.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan 2005.

7. Notwithstanding the submitted plans the development hereby approved shall not be first occupied unless and until the proposed vehicular access to Ruden Way has been constructed and provided with dropped kerbs and tactile paving at the pedestrian crossing points and pedestrian visibility of 2 metres back from the rear of the footway by 2 metres from the sides of the access in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority. No fence, wall or other obstruction to visibility between 0.6m and 2m in height above ground level shall be erected within the area of the visibility.

Reason:

The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and Policy Mo5 highway safety of the Reigate and Banstead Local Plan 2005.

8. The development hereby approved shall not be occupied unless and until the existing accesses from the site to Ruden Way have been permanently closed and any kerbs, verge, footway, fully reinstated.

Reason:

The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and Policy Mo5 highway safety of the Reigate and Banstead Local Plan 2005.

9. No development shall commence until a Construction Transport Management Plan, to include details of:
 - (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) provision of boundary hoarding behind any visibility zones
 - (e) measures to prevent the deposit of materials on the highway
 - (f) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
 - (g) on-site turning for construction vehicles has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason:

The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and Policy M05 highway safety and policy Mo6 Turning Space of the Reigate and Banstead Local Plan 2005.

10. No development shall commence until a scheme for the disposal of foul and surface water drainage from the site has been submitted to and approved by

the Local Planning Authority. The scheme shall be implemented as approved prior to the occupation of the dwelling hereby permitted.

Reason:

To ensure that the site is satisfactorily drained with regard to Reigate and Banstead Borough Local Plan 2005 policies Ut3 and National Planning Policy Framework.

11. No development shall take place until a scheme for noise mitigation measures for the dwellings has been submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details before the dwellings hereby permitted are first occupied.

Reason:

To protect the occupants of the proposed development from noise disturbance with regard to Reigate and Banstead Borough Local Plan 2005 policy Ho9.

12. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected, including provision for wildlife access, has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason:

To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Pc4.

13. The development hereby approved shall not be occupied until the proposed refuse collection point has been provided in accordance with the approved plans. Thereafter the refuse collection point shall be retained and maintained for its designated purpose.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users. The above condition is required in order to meet the objectives of the NPPF (2012), and to satisfy policy Mo7 of the Reigate and Banstead Borough Local Plan (2005).

14. No development shall take place until bat surveys have been completed and a report containing the results and details of appropriate mitigation measures have been submitted to and approved in writing by the Council.

Reason:

To protect the important species on the site in accordance with Policy Pc2G of the Reigate and Banstead Borough Local Plan 2005, Natural England standing advice and the provisions of the NPPF

15. The first floor windows in the north east and south west side elevations of the development hereby permitted shall be glazed with obscured glass which shall be fixed shut, apart from a top hung opening fanlight whose cill height shall not be less than 1.7 metres above internal floor level, and shall be maintained as such at all times.

Reason:

To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Borough Local Plan 2005 policy Ho9.

16. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995, (or any Order revoking and re-enacting that Order with or without modification), no first floor windows, dormer windows or rooflights other than those expressly authorised by this permission shall be constructed.

Reason:

To ensure that the development does not affect the amenity of the neighbouring property by overlooking and to protect the visual amenities of the area in accordance with Reigate and Banstead Borough Local Plan 2005 policy Ho9.

17. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no extensions permitted by Classes A B and C of Part 1 of the Second Schedule of the 2015 Order shall be constructed.

Reason:

To control any subsequent enlargements in the interests of the visual and residential amenities of the locality with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9, Ho13, and Ho16

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, a 140 litre wheeled bin conforming to British Standard BSEN840 and a 60 litre recycling box should be provided for the exclusive use of the occupants of that dwelling. Prior to the initial occupation of any communal dwellings or flats, wheeled refuse bins conforming to British Standard BSEN840, separate recycling bins for paper/card and mixed cans, and storage facilities for the bins should be installed by the developer prior to the initial occupation of any dwelling hereby permitted. Further details on the required number and specification of wheeled bins and recycling boxes is available from the Council's Neighbourhood Services on 01737 276501 or 01737 276097, or on the Council's website at www.reigate-banstead.gov.uk. Bins and boxes meeting the specification may be purchased from any appropriate source, including the Council's Neighbourhood Services Unit on 01737 276775.

4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
- (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowzers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

5. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
6. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. The planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate substantial sized trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Advanced Nursery Stock size with initial planting heights of not less than 4.5m, with girth measurements at 1m above ground level in excess of 16/18cm.

7. The use of a suitably qualified arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural tree condition above. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
8. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
9. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding or any other device or apparatus for which a licence must be sought from the Highway Authority Local Highways Service.
10. The permission hereby granted shall not be construed as authority to carry out any works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/floodingadvice.
11. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
12. When a temporary access is approved or an access is to be closed as a condition of planning permission an agreement with, or licence issued by, the Highway Authority Local Highways Service will require that the redundant dropped kerb be raised and any verge or footway crossing be reinstated to conform with the existing adjoining surfaces at the developers expense.
13. The developer is advised that as part of the detailed design of the highway works required within the highway to accommodate the proposed access, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers,

street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.

14. A pedestrian inter-visibility splay of 2m by 2m shall be provided on each side of the access, the depth measured from the back of the footway and the widths outwards from the edges of the access. No fence, wall or other obstruction to visibility between 0.6m and 2m in height above ground level shall be erected within the area of such splays.
15. Anecdotal evidence suggests that properties in the locality may have been damaged via aerial delivered ordnance during WWII. Therefore there is the potential for the presence of Unexploded Ordnance (UXO) to be present beneath part(s) of the site. Groundworkers should be made aware of this so suitable mitigation measures and groundworker awareness is in place prior to commencement of any intrusive works. Should any suspect or actual UXO be identified during intrusive works, works should cease immediately and the Local Police and Environmental Protection (for information purposes) should be contacted immediately for further guidance. CIRIA C681: UXO a Guide to the Construction Industry (Guidance Document) can provide further information on UXO matters relating to construction.
16. The applicant is advised to consider the advice of Thames Water in their response dated 7th January 2019 regarding the sewer requirements in this area.

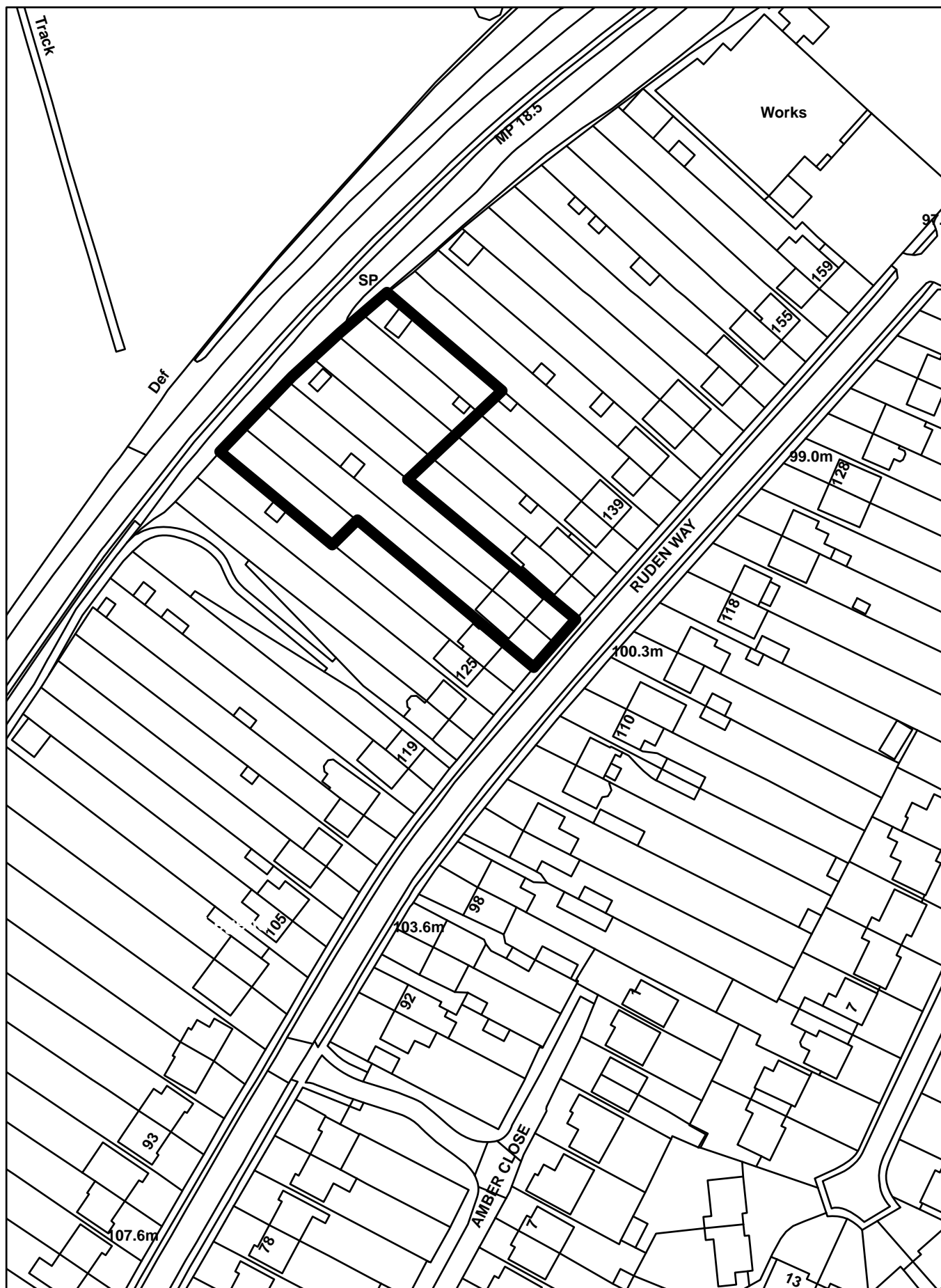
REASON FOR PERMISSION

The development hereby permitted has been assessed against development plan policies Pc2G , Pc4, Ho9, Ho13, Ho14, Ho16, Mo5, Mo7, Ut3 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

18/01920/F - Land Rear Of 127-139 Ruden Way, Epsom Downs





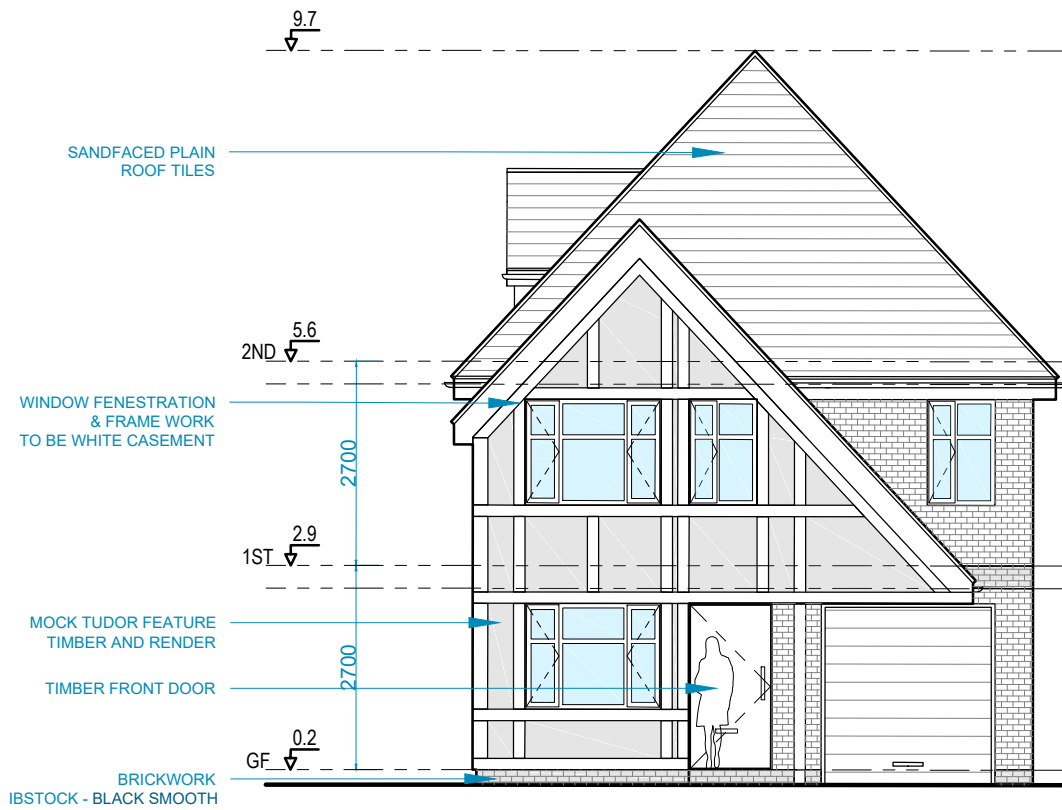
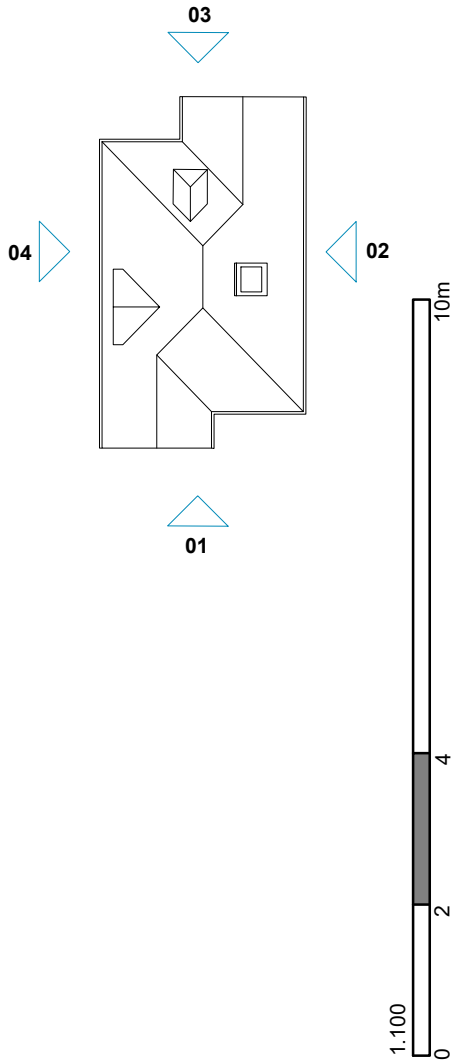
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- DO NOT SCALE FROM THIS DRAWINGS
- SETTING OUT & ALL MEASUREMENT SHOULD BE
TAKEN ON SITE

Client. HARNEL GROUP	
Project. LAND TO THE REAR OF 127 - 139 RUDEN WAY EPSOM KT17 3LW	
Title. PROPOSED DRAWINGS REVISED SITE PLAN	
Project Initial. RW	Date. 28 NOVEMBER 2018
Drawing Number. RW_PA3 100	Revision. A
Drawing Scale. 1:500 @ A3	Drawn. AG
	Checked. AJ

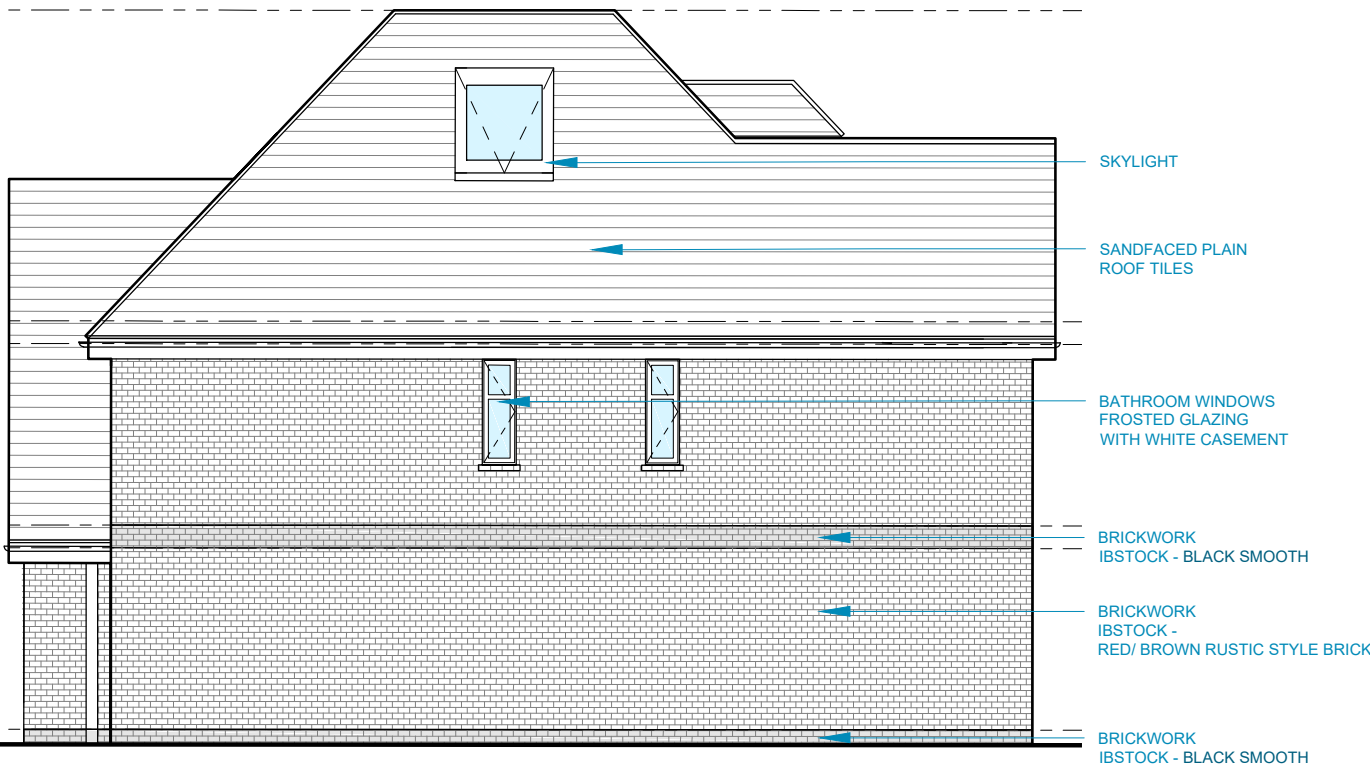
REVISED SITE PLAN. PROPOSED

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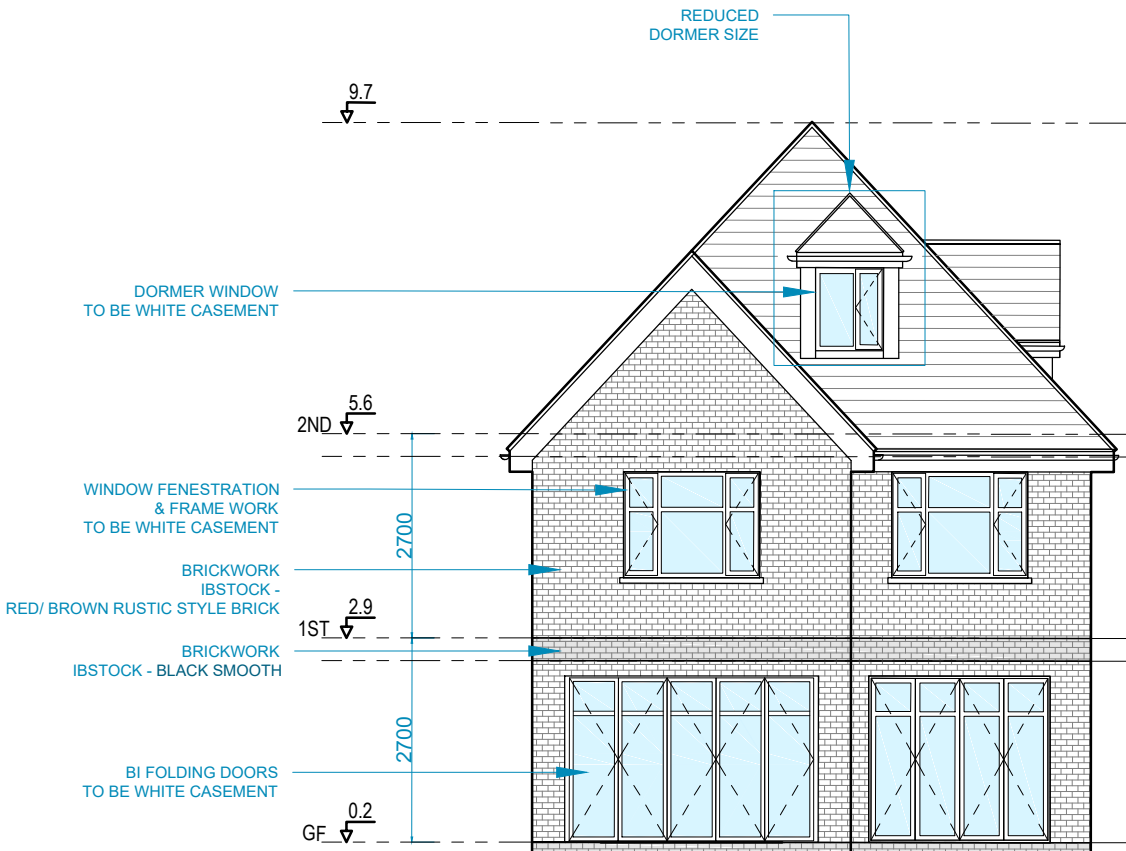
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SHOULD BE TAKEN ON SITE



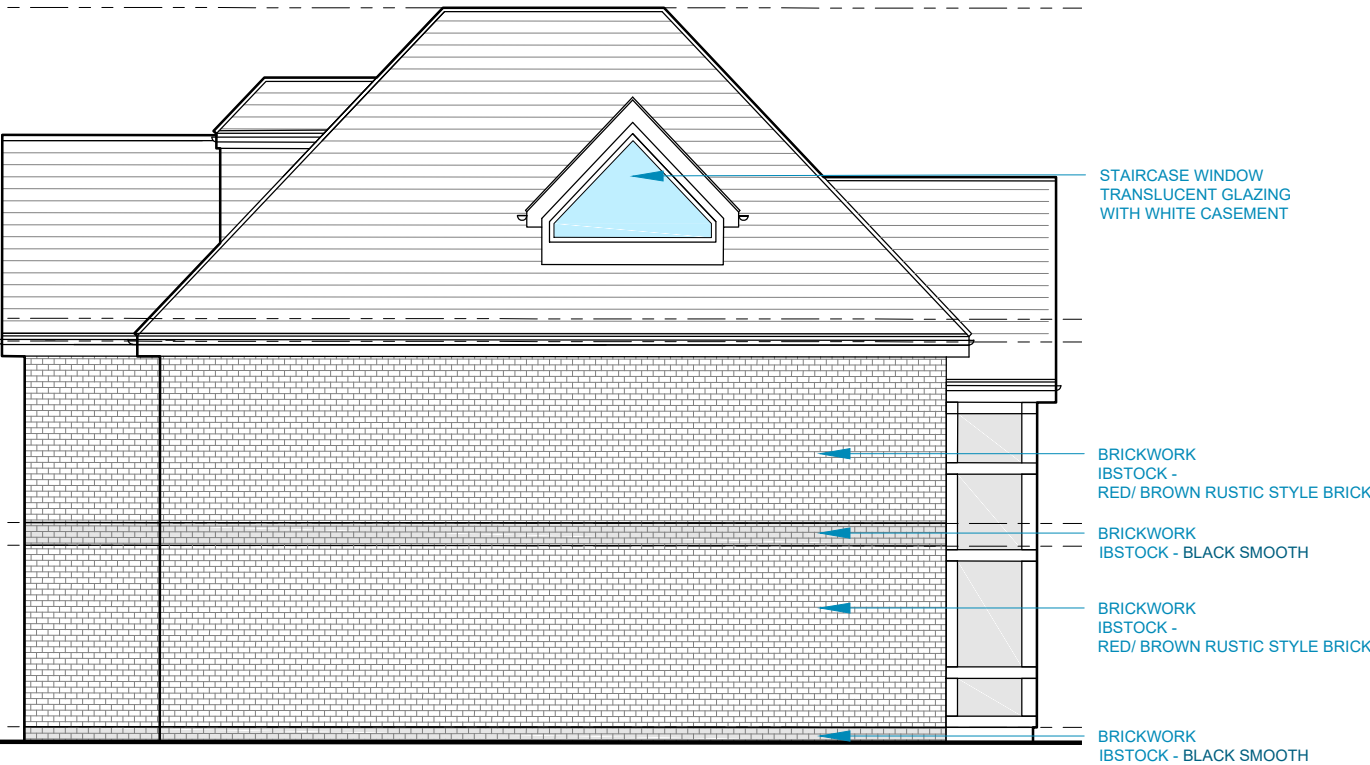
ELEVATION 01. HOUSE TYPE 01 - 160 SQM



ELEVATION 02. HOUSE TYPE 01 - 160 SQM



ELEVATION 03. HOUSE TYPE 01 - 160 SQM



ELEVATION 04. HOUSE TYPE 01 - 160 SQM

Client.

HARNEL GROUP

Project.

**LAND TO THE REAR OF
127- 139 RUDEN WAY
EPSOM
KT17 3LW**

Title.

**PROPOSED ELEVATIONS - TYPE 01
5 BEDROOM HOUSE - 160 SQM**

Project Initial.

RW

Date.

09 JANUARY 2018

Drawing Number.

RW_PA2 102

Revision.

B

Drawing Scale.

1:100 @ A3

Drawn.

JS

Checked.

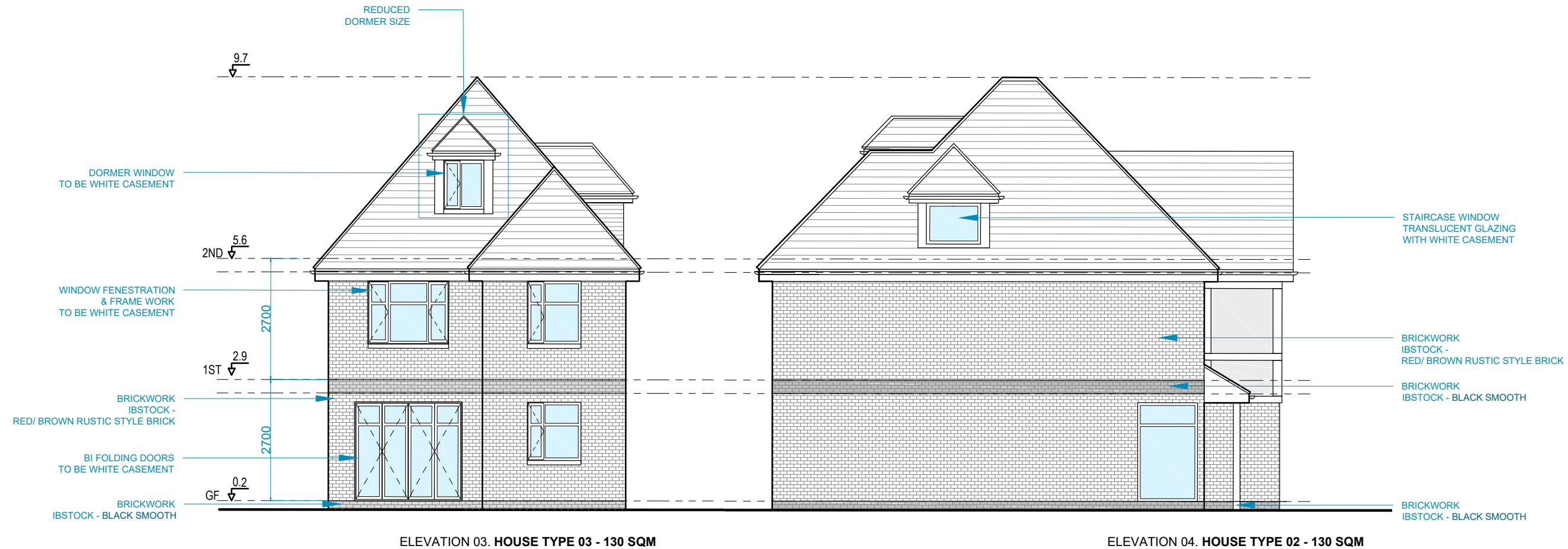
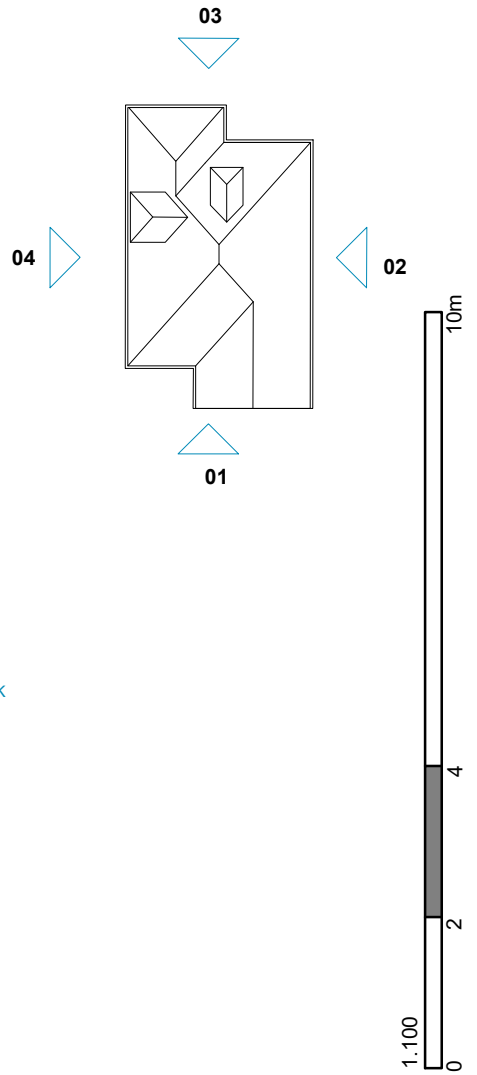
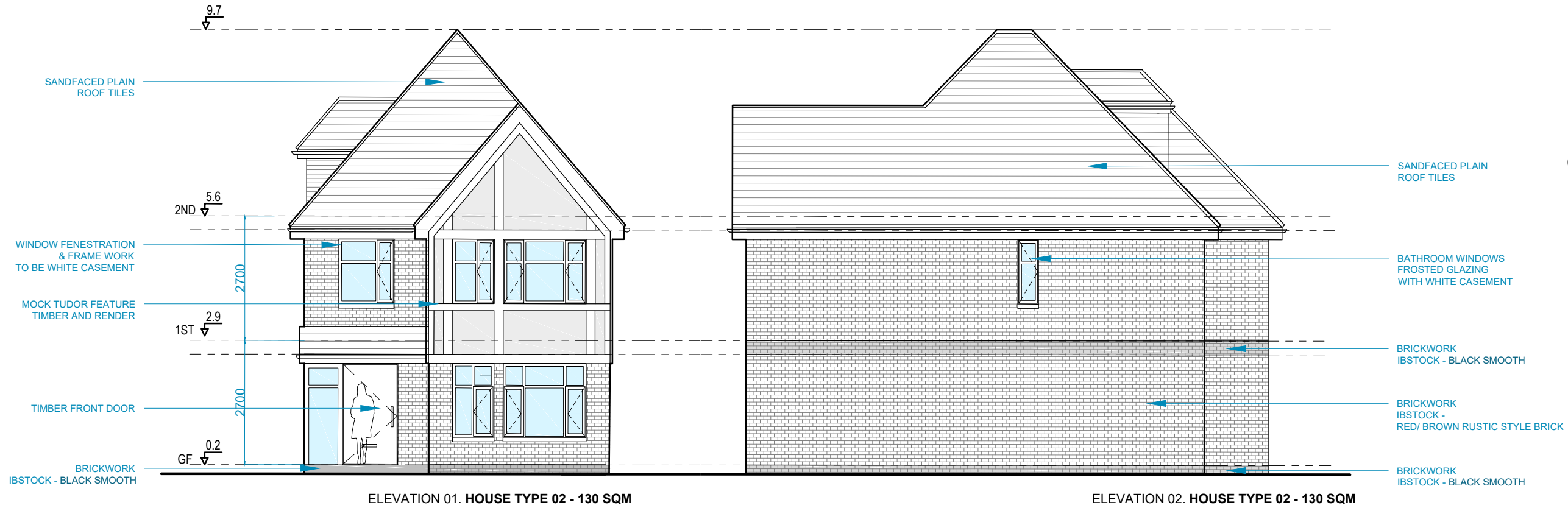
AJ

AQUA

Architecture | Urban Design | Interiors | Building Consultancy
ONE Alfred Place, 1 Alfred Place, London, UK
info@aquaarchitects.com | www.aquaarchitects.com

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Client.

HARNEL GROUP

Project.

**LAND TO THE REAR OF
127- 139 RUDEN WAY
EPSOM
KT17 3LW**

Title.

**PROPOSED ELEVATIONS - TYPE 02
5 BEDROOM HOUSE - 130 SQM**

Project Initial.

RW

Date.

08 January 2018

Drawing Number.

RW_PA2 202

Revision.

B

Drawing Scale.

1:100 @ A3

Drawn.

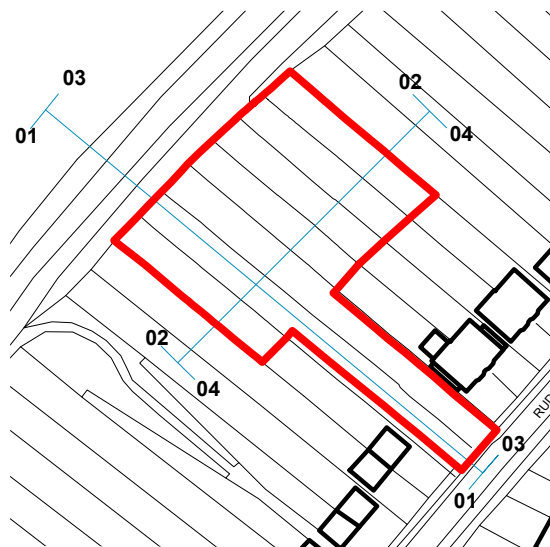
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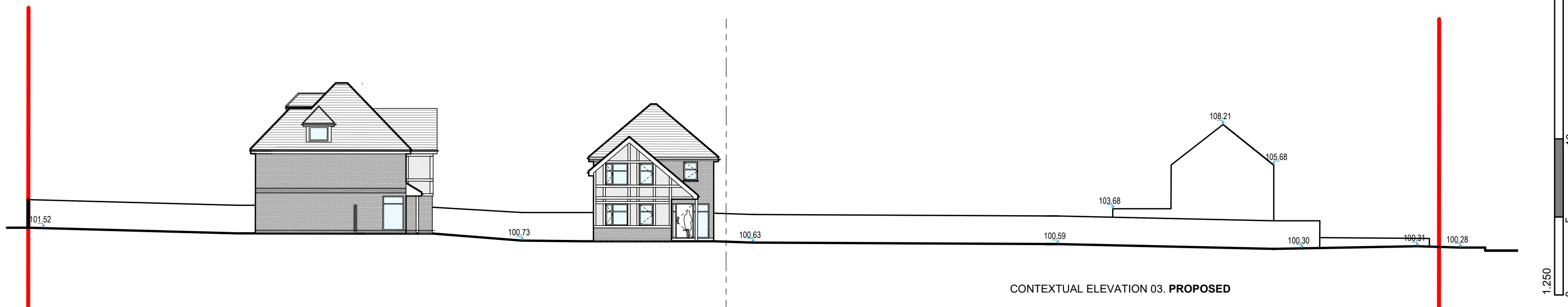
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AQUA

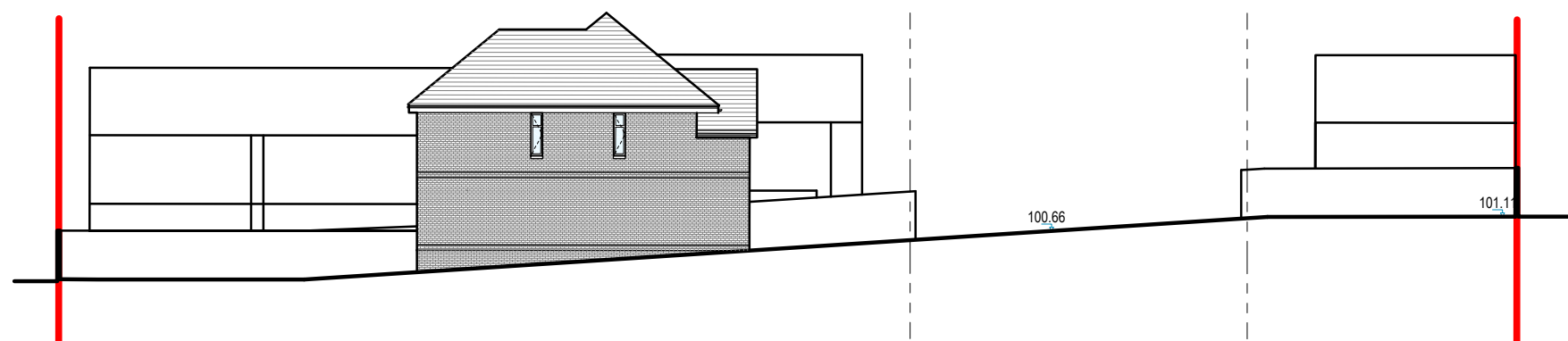
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ONE Alfred Place, 1 Alfred Place, London, UK
info@aquaarchitects.com | www.aquaarchitects.com



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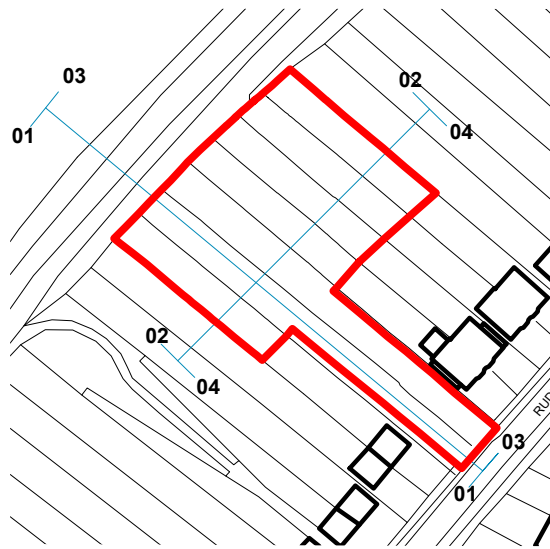
CONTEXTUAL ELEVATION 03. PROPOSED



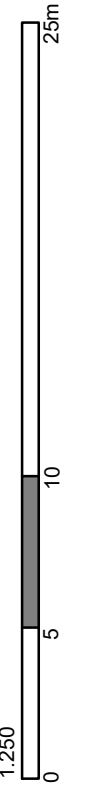
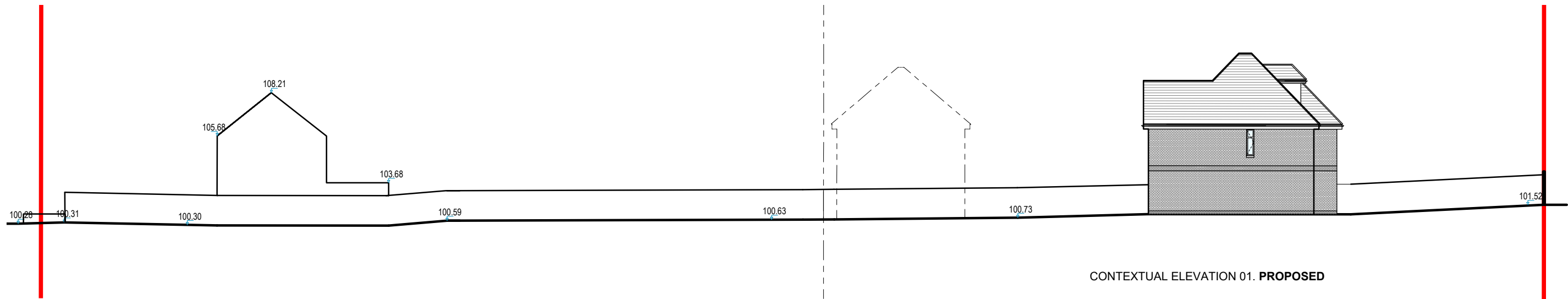
CONTEXTUAL ELEVATION 04. PROPOSED

Client. HARNEL GROUP		
Project. LAND TO THE REAR OF 127 - 139 RUDEN WAY EPSOM KT17 3LW		
Title. PROPOSED CONTEXTUAL ELEVATIONS ELEVATIONS 03 + 04		
Project Initials. RW	Date. 06 SEPTEMBER 2018	
Drawing Number. RW_PPA 402	Revision.	
Drawing Scale. 1:250 @ A3	Drawn. JS	Checked. AJ



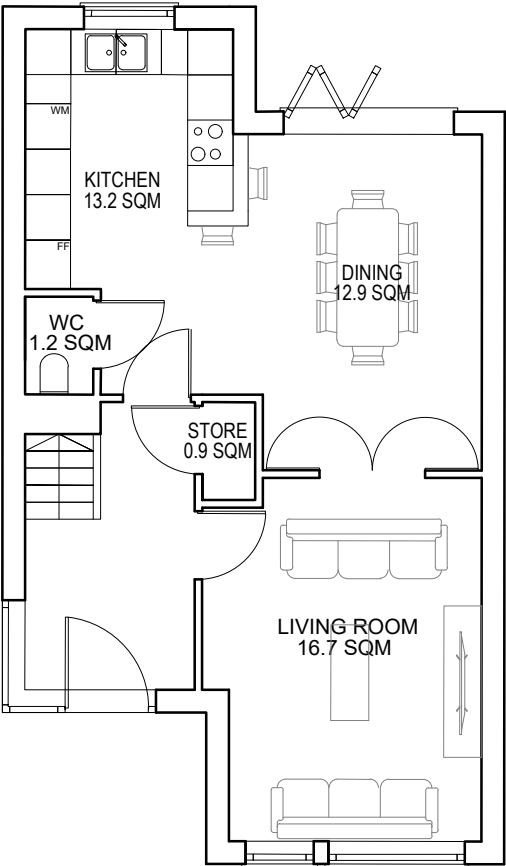


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 - SETTING OUT & ALL MEASUREMENT SHOULD BE TAKEN ON SITE

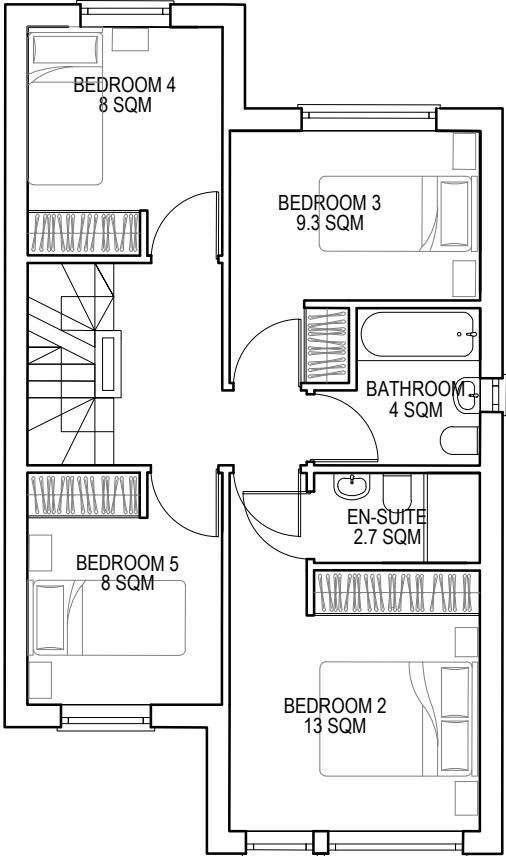


Client. HARNEL GROUP		
Project. LAND TO THE REAR OF 127 - 139 RUDEN WAY EPSOM KT17 3LW		
Title. PROPOSED CONTEXTUAL ELEVATIONS ELEVATIONS 01 + 02		
Project Initials. RW	Date. 06 SEPTEMBER 2018	
Drawing Number. RW_PA2 401	Revision.	
Drawing Scale. 1:250 @ A3	Drawn. JS	Checked. AJ

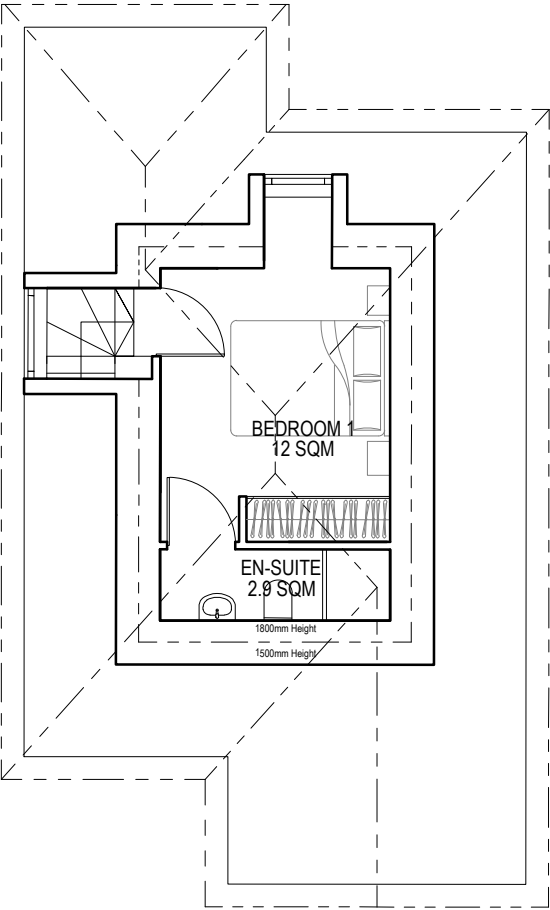




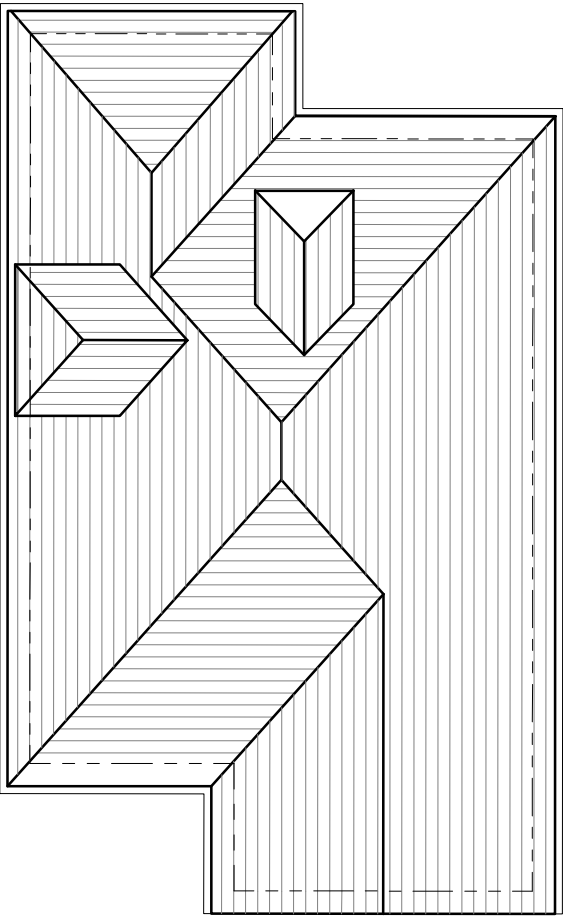
GROUND FLOOR PLAN. HOUSE TYPE 02 - 5 BED 130 SQM



FIRST FLOOR PLAN. HOUSE TYPE 02 - 5 BED 130 SQM

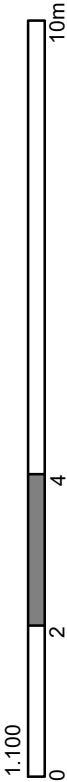


MANSARD FLOOR PLAN. HOUSE TYPE 02 - 5 BED 130 SQM



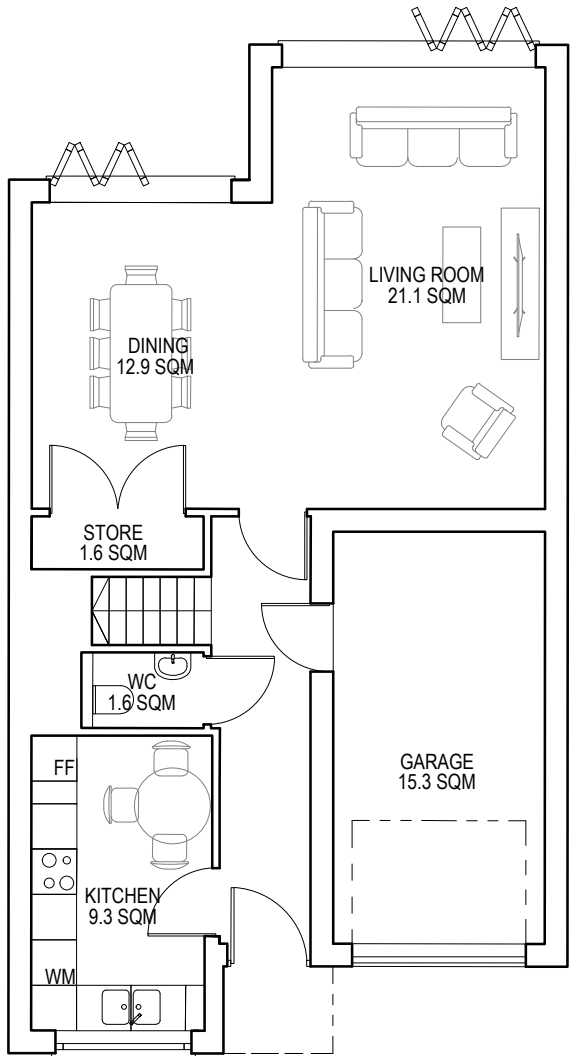
ROOF PLAN. HOUSE TYPE 02 - 5 BED 130 SQM

Notes.
- DO NOT SCALE FROM THIS DRAWINGS
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SHOULD BE TAKEN ON SITE

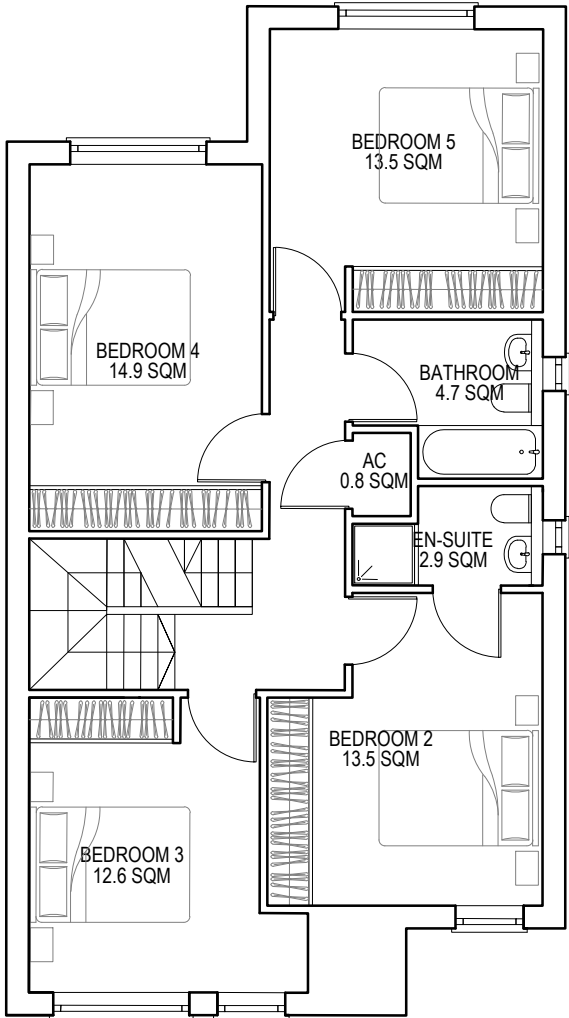


Client. HARNEL GROUP		
Project. LAND TO THE REAR OF 127- 139 RUDEN WAY EPSOM KT17 3LW		
Title. PROPOSED FLOOR PLAN - TYPE 02 5 BEDROOM HOUSE - 130 SQM		
Project Initial. RW	Date. 07 January 2018	
Drawing Number. RW_PA2 201	Revision. A	
Drawing Scale. 1:100 @ A3	Drawn. AG	Checked. AJ

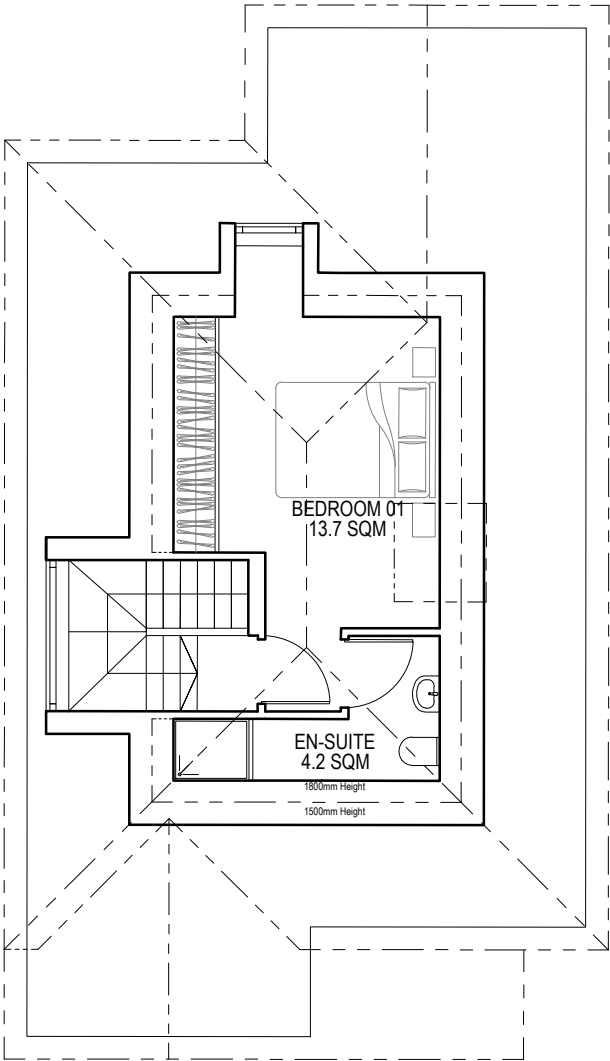




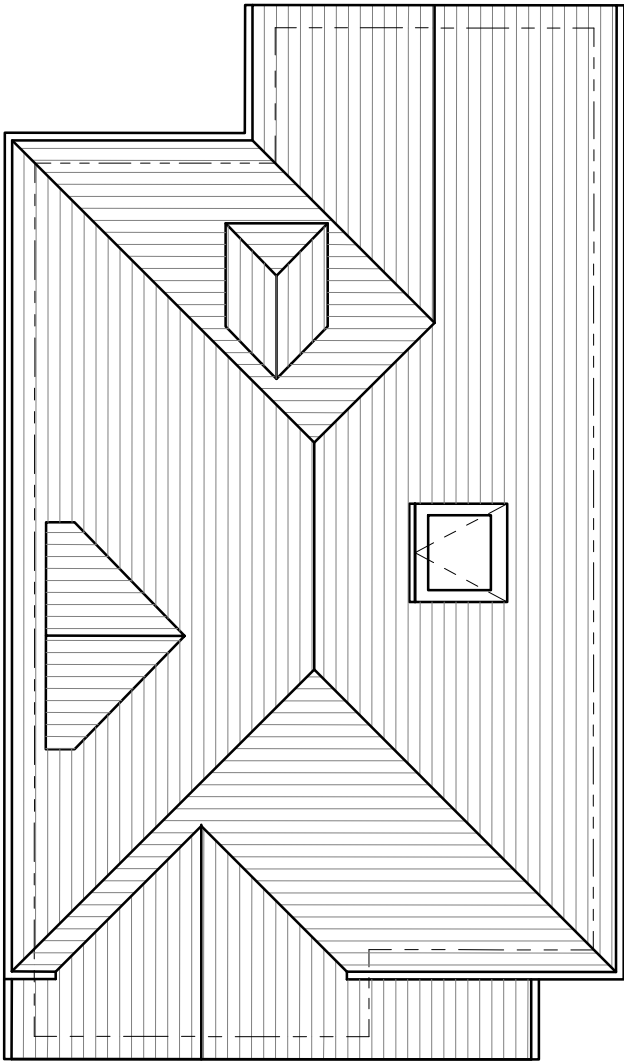
GROUND FLOOR PLAN. HOUSE TYPE 01 - 160 SQM



FIRST FLOOR PLAN. HOUSE TYPE 01 - 160 SQM

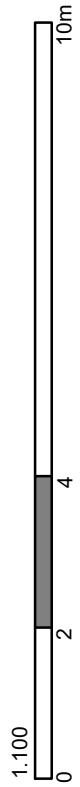


LOFT FLOOR PLAN. HOUSE TYPE 01 - 160 SQM



ROOF FLOOR PLAN. HOUSE TYPE 01 - 160 SQM

Notes.
- DO NOT SCALE FROM THIS DRAWINGS
- SETTING OUT & ALL MEASUREMENT
SHOULD BE TAKEN ON SITE



Client. HARNEL GROUP		
Project. LAND TO THE REAR OF 127- 139 RUDEN WAY EPSOM KT17 3LW		
Title. PROPOSED FLOOR PLANS - TYPE 01 5 BEDROOM HOUSE - 160 SQM		
Project Initial. RW	Date. 07 Jan 2018	
Drawing Number. RW_PA2 101	Revision. A	
Drawing Scale. 1:100 @ A3	Drawn. JS	Checked. AJ





PROPOSED SITE - VISUAL 02

Notes.
- DO NOT SCALE FROM THIS DRAWINGS
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SHOULD BE TAKEN ON SITE

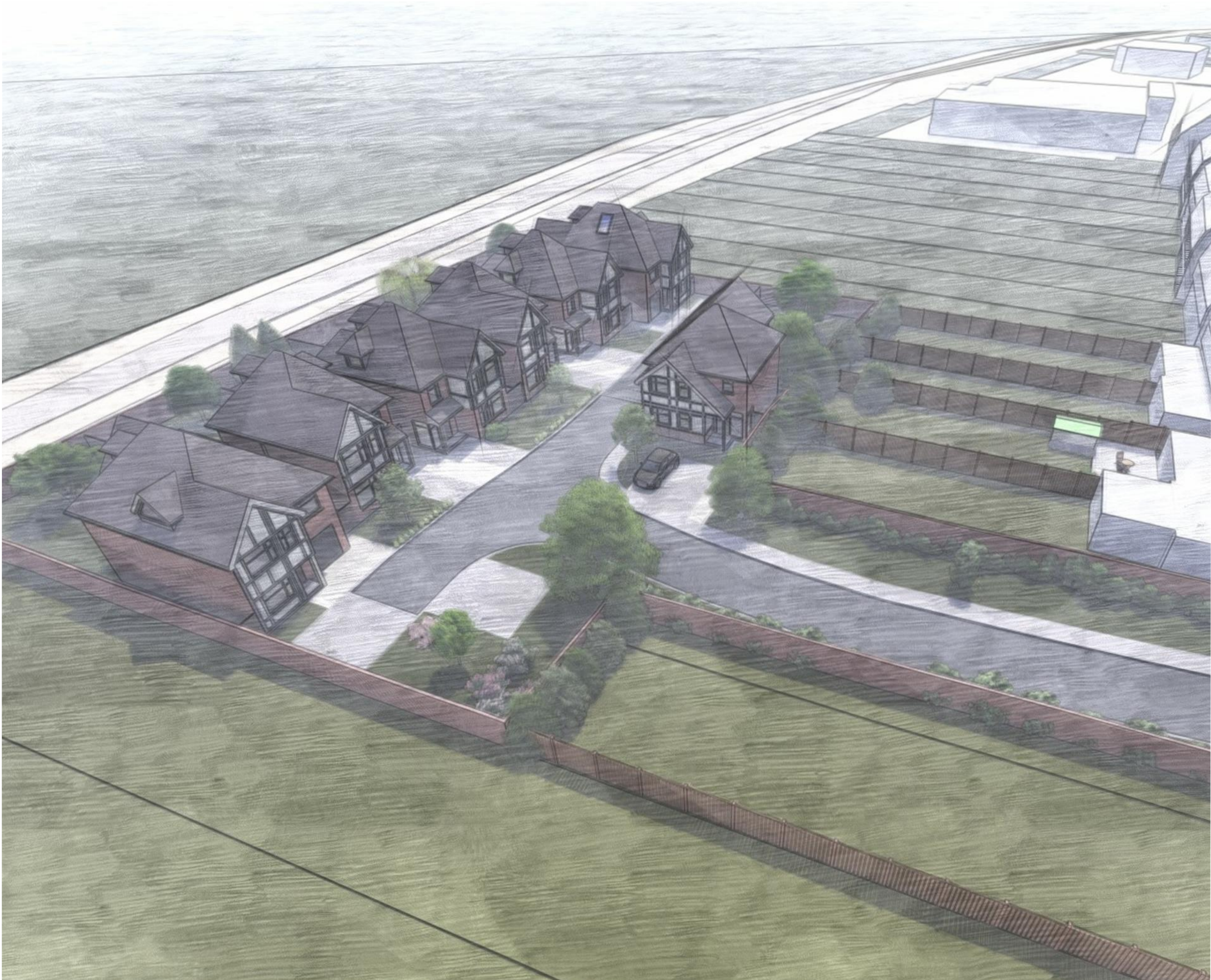
Client.
HARNEL GROUP

Project.
**LAND TO THE REAR OF
127- 139 RUDEN WAY
EPSOM
KT17 3LW**

Title.
PROPOSED SITE VISUAL 02

Project Initial.	Date.	
RW	06 SEPTEMBER 2018	
Drawing Number.	Revision.	
RW_PA2 502		
Drawing Scale.	Drawn.	Checked.
1:100 @ A3	AG	AJ





PROPOSED SITE - VISUAL 01

Notes.
- DO NOT SCALE FROM THIS DRAWINGS
- SETTING OUT & ALL MEASUREMENT
SHOULD BE TAKEN ON SITE

Client.
HARNEL GROUP

Project.
**LAND TO THE REAR OF
127- 139 RUDEN WAY
EPSOM
KT17 3LW**

Title.
PROPOSED SITE VISUAL 01

Project Initial.	Date.	
RW	06 SEPTEMBER 2018	
Drawing Number.	Revision.	
RW_PA2 501		
Drawing Scale.	Drawn.	Checked.
1:100 @ A3	AG	AJ

